

South Sunderland Growth Area

Supplementary Planning Document

June 2020



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1 Introduction

1.1 Background

Sunderland's Local Plan is in three parts:

Part One – Core Strategy and Development Plan (CSDP)

This Plan sets an overarching strategy, strategic policies and strategic allocations and designations for the future change and growth of Sunderland. This Plan also includes local policies for development management purposes. This Plan covers the period from 2015 to 2033 and covers all land within Sunderland's administrative boundaries.

The plan was adopted in January 2020.

Part Two – Allocations and Designations Plan (A+D)

This will set out local policies including site-specific policy designations and allocations for the development, protection and conservation of land in the city in order to deliver the overall strategy set out within this Plan. This Plan covers all land within Sunderland's administrative boundaries.

Part Three – International Advanced Manufacturing Park (IAMP) Area Action Plan (AAP) 2017-203

This Plan was adopted by Sunderland City Council and South Tyneside Council in November 2017. This part of the Local Plan sets out site specific policies for the comprehensive development of the IAMP.

The Local Plan will supersede saved policies of the Sunderland Unitary Development Plan (UDP) 1998 and UDP Alteration No. 2 (2007). However, a number of policies will remain as saved policies and part of the Development Plan until such time as the A&D Plan is adopted. These saved policies will continue to be applied and be a consideration in the determination of planning applications, until they are replaced by policies in the A&D Plan.

The CSDP allocates a new growth area known as South Sunderland Growth Area (SSGA) which comprises 227ha of land in the Ryhope and Doxford Park wards. The SSGA is identified as a major new growth area which will create a new high quality vibrant and distinctive neighbourhood.

The SSGA is to be allocated for approximately 3000 new homes, meeting approximately 20% of the City's future housing need and, in particular, provide significant numbers of much needed larger family homes. It is considered that delivery of the SSGA will contribute to achieving the strategic aims of the Council's City Plan and Sunderland Strategy (2008 – 2025) and assist in meeting the Council's 3,6,9 Vision.

1.2 Purpose

Due to development pressure on individual sites within SSGA, it has been considered necessary to prepare a SPD to provide further guidance to help inform the formulation and assessment of individual proposals for development within SSGA. In particular, it is important that in the case of each development proposals within SSGA the cumulative impacts of the development proposals are considered, including in relation to overall provision and environmental protection requirements. In addition, the SPD seeks to coordinate potential development proposals to ensure the creation of a sustainable community, in a good quality built and natural environment, with access to local services that reflect the community's needs and support its health and well-being.

The SPD provides more information about SSGA and its context, sets out the Council's visions and aspirations and makes clear what the Council's expectations are for any further development proposals to ensure that SSGA is a high quality, vibrant and distinctive neighbourhood which is well integrated with surrounding communities and existing residential properties. This document comprehensively covers key strategic issues such as design standards, infrastructure delivery, flood risk, environmental management.

Development proposals coming forward in SSGA should be considered against the Local Plan, this draft SPD (and various supporting evidence bases that have informed the SPD) together with all other relevant material planning considerations. A list of all the relevant evidence bases can be found in Appendix 1.

Further details of the background to the SPD are set out in later sections of this document.

1.3 Scope

The SPD along with supporting documents will:

- Define key development principles and concepts to enable a strategic approach to delivering the proposed level of growth;
- Identify requirements and provide a broad strategy to deliver supporting infrastructure;
- Provide a masterplan framework, incorporating design parameters and principles to ensure a high standard of design and sustainability; and
- Demonstrate that the Growth Area will not have 'Likely Significant Effects' on the European Designations at the coast and accord with the terms of the Habitat Directives.

1.4 Sustainability appraisal

A Sustainability Appraisal (SA) has been carried out for the South Sunderland Growth Area SPD. This fulfils the requirements of the SEA Directive, but also ensures that the social and economic impacts of the SPD are fully understood. The SA that has been prepared for SSGA fits within the framework established by the SA undertaken to inform preparation of the Local Plan.

The SA Scoping Report for the SPD was published for consultation with statutory agencies in April/May 2014. This process sought to confirm that the scope of the assessment was appropriate and adequate to assess and inform preparation of the SPD.

Following confirmation that the scope of the SA was appropriate for the SPD the SA report was then produced to inform the approach taken forward in this SPD.

The SA Report was published for consultation alongside the initial draft SPD and no comments were raised.

1.5 Habitat regulations

Assessment

Sunderland has a number of European Designations within its boundary; as such it is necessary to take into account the Habitat Directive during the plan making process. The Habitat Regulations Assessment (HRA) is in response to the Habitat Directive and requires plans and projects to undertake, in the first instance, an assessment of whether proposals are likely to result in

'Likely Significant Effects' (LSEs) on the designations. If LSEs (including in-combination) are deemed unlikely,

then no further assessment is necessary. If, on the other hand, significant effects are likely, or there is any doubt, then the HRA must continue to a more detailed second stage called Appropriate Assessment/Full HRA. An Appropriate Assessment/Full HRA typically establishes mitigation and/or combination measures and must ascertain with no reasonable scientific doubt, that the integrity of the European designations will not be adversely affected.

A screening report was undertaken in April/May 2014 to identify whether the SPD was likely to result in LSEs. The screening report concluded that it was not possible to rule out LSEs on the Northumbria Special Protection Area (SPA), Teesdale & Cleveland SPA and Durham Coast Special Areas of Conservation (SAC) and as such mitigation measures would be required. For this reason it was deemed necessary to proceed to the second stage of the Habitat Regulations Assessment process and an Appropriate Assessment was undertaken.

The HRA has informed the approach being taken forward in this SPD, identified and discussed further in Chapter 7. In summary, the HRA concludes that if the proposed mitigation measures are implemented then the development of SSGA will not have LSEs on the nearby SPA and SAC.

1.6 Partnership

The SPD has been developed in consultation with the relevant landowners/developers, relevant stakeholders and the local community.

1.7 Consultation

Upon first drafting of the SPD initial pre-consultation was undertaken with the local community in July 2015 to raise awareness of the council's intentions for SSGA and to seek the public's views. The draft SPD produced in January 2016 was informed by comments received as part of these pre-consultation events.

Formal consultation was undertaken on the initial draft SPD for 8 weeks in 2017, between 7 August and 2 October 2017 and included neighbour consultation letters, e-mails, public exhibitions, statutory advertisement and press releases.

A number of responses were received to the SPD and where possible changes have been made to the document to reflect concerns¹.

¹ A full record of all representations received as part of the consultation and the council's response are set out within the SSGA consultation statement February 2020.

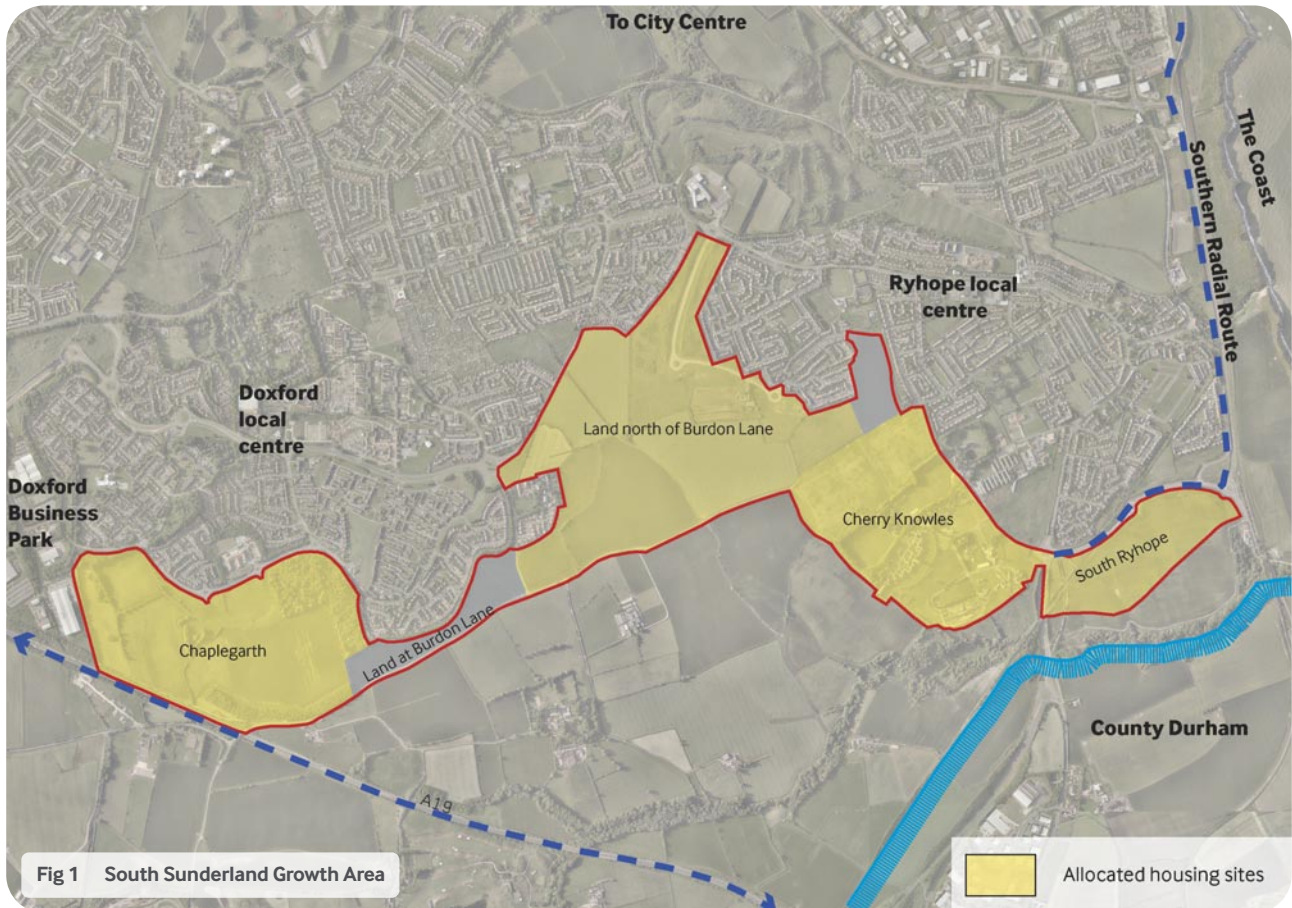
In February 2020 the SPD was published for a further four week public consultation to allow the wider public and interested groups to comment on changes that have been made to the document since the last draft was published. These changes include the outcome of discussions of the SSGA at the Examination in Public and to reflect Policy SS6 of the adopted CSDP.

Where necessary the final SPD has been updated to take account of the representations received as part of this consultation.

1.8 Future proofing

In order to guarantee the SPD is relevant throughout the project life-cycle, it has to be flexible and capable of responding to changes in people's lifestyles and environment as well as commercial and economic circumstances. This is particularly relevant as the SSGA will be planned and constructed over approximately 15-20 years.

2 Study area - in brief



2.1 South Sunderland Growth Area

The study area is a 277ha site located less than 3 miles south of Sunderland city centre and 9 miles north east of Durham city centre. It is bounded to the south by designated Green Belt, to the east by the coast, to the north by residential development and to the west by the A19 and Doxford Business Park.

Work has started on developing parts of the study area for new housing, however parts are still arable farmland located in an attractive green setting, bounded by established natural features including Blakeney Woods, Cherry Knowle Dene, Ryhope Dene and the Green Belt. The study area has many desirable qualities affording impressive views across the city and coast, being easily accessible from Doxford Business Park and the wider region via the A19.

The growth area covers parts of two wards: Ryhope to the east and Doxford to the west. SSGA can be divided into four areas collectively known as the South Sunderland Growth Area – these comprise of:

- Chapelgarth
- Land North of Burdon Lane
- Cherry Knowle
- South Ryhope

Chapelgarth

Chapelgarth is a 45ha greenfield site. The site has a strategic role to play in meeting the City's need for a diversified housing stock, having been identified in the Unitary Development Plan as suitable for executive housing. Part of the site is owned by Miller Homes, who are delivering the first phase housing development, which is for 160 dwellings. Work is well underway with this first phase, with a handful of dwellings already complete as of Autumn 2019. The remainder of the site, which is owned by Siglion will be developed out in multiple phases by house builders.

Land north of Burdon Lane

Land to the North of Burdon Lane is a 84ha greenfield site allocated for residential development within the CSDP. The area is in the ownership of a number of land owners. A consortium of developers has been established to deliver the future development in this area and has the benefit of a number of option agreements in respect of the area.

Cherry Knowle

Cherry Knowle Hospital is a mixed brownfield/greenfield site which includes an area of Green Belt and Open Countryside, extending to approximately 47 ha. The site will provide 800 new dwellings, of which Barratts/David Wilson homes are delivering the first phase comprising of 304 dwellings, with a significant number of completions to date. The remainder of the site is still in the ownership of Homes England who are looking to identify developers to deliver remaining phases.

South Ryhope

South Ryhope is a 36ha greenfield site on the edge of the growth area. The site is in the ownership of the Church Commissioners.

The CSDP allocates the site for residential development and the site has the benefit of Outline Planning consent for up to 450 dwellings.

2.2 Neighbouring sites

There are a number of other smaller sites either within or on the periphery of the SSGA that are also discussed within the SPD. These are:

- Rushford Phase 2
- Mill Hill/Silksworth Lane
- Land at Burdon Lane

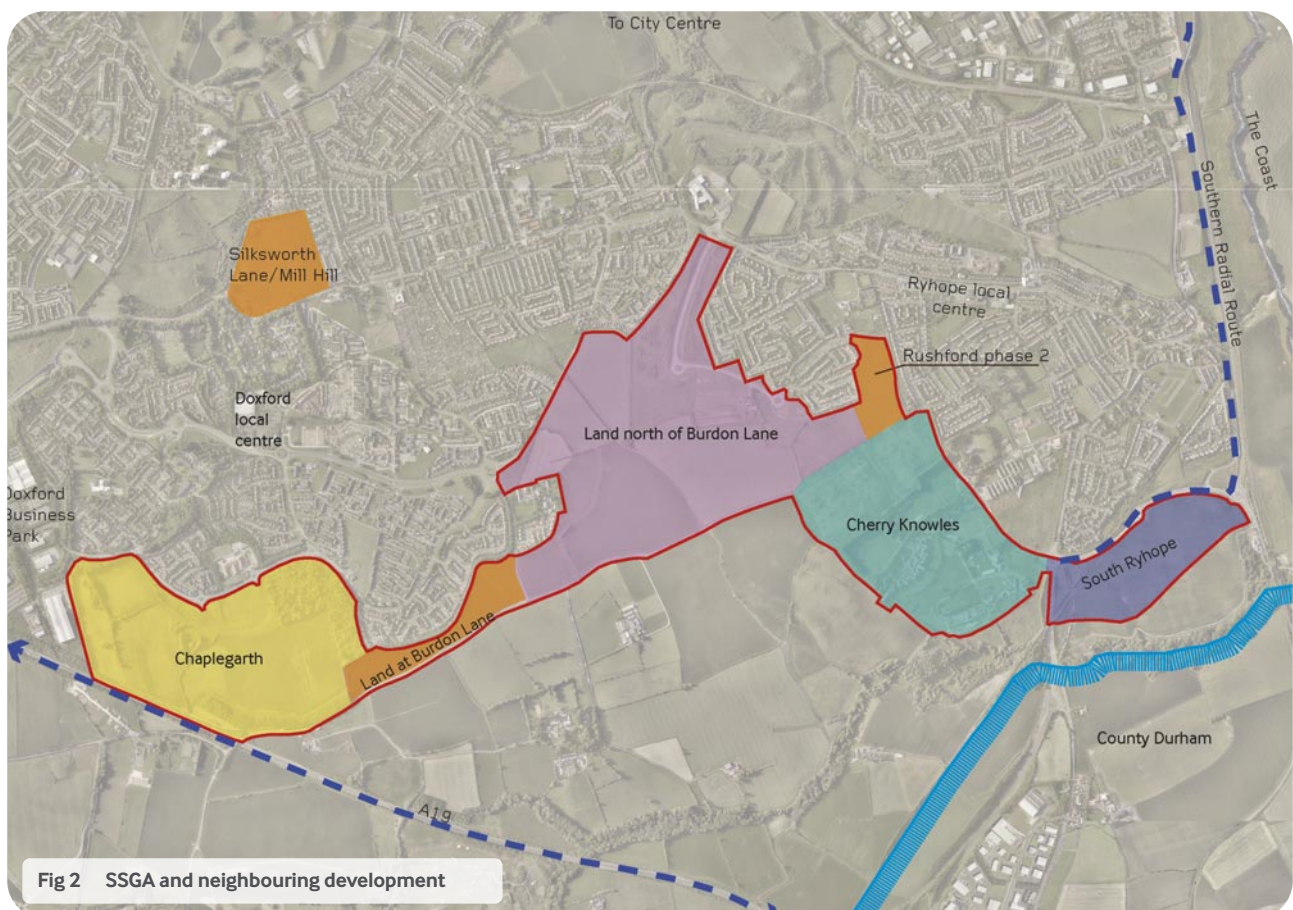


Fig 2 SSGA and neighbouring development

3 Policy background

3.1 Introduction

Sunderland is losing population through net out-migration. The council's Strategic Housing Market Assessment (SHMA) identifies a clear need to provide housing stock which better meets demand and household aspirations to stem this net out-flow and also encourage new households in to the city.



Strategic local policy such as the City Plan 2019-2030 and the Sunderland Strategy (2008-2025) emphasises the need to encourage the retention and attraction of new households by improving housing for residents and providing an increased proportion of higher value housing.

To ensure land availability is not a constraint on the provision of housing stock the council undertakes regular Strategic Housing Land Availability Assessments (SHLAA) to assess sites for their housing potential and development timescale. The SHLAA (2019) identifies sites within the wider Sunderland South sub-area as having the potential to deliver around 43% of the city's housing requirement, which will have a key role in meeting housing demand in the city.

The CSPD allocates strategic sites, which are seen as fundamental to the delivery of the plan. The South Sunderland Growth Area is a Strategic Site due to its potential to deliver approximately 3,000 new dwellings over the next 15-20 years. This equates to approximately 20% of the city's housing need and will provide much needed larger family homes. As such, SSGA has a strategic role to play in meeting the city's housing needs in terms of both quantity and choice.

3.2 European

Habitats Directive

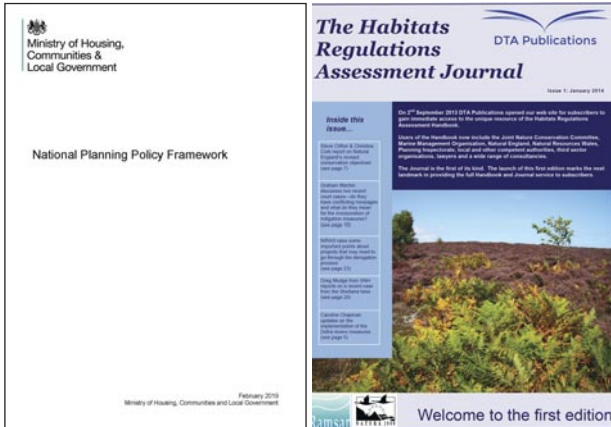
As a member of the European Union, the UK is bound by the terms of the Council Directive 79/409/EEC on the Conservation of Wild Birds (the Birds Directive) and the Council Directive 92/43/EEC on the conservation of natural habitats and wild flora and fauna (the Habitats Directive). These are implemented in the UK through the Conservation Regulations which provide for the protection of areas of European importance for wildlife, in the form of Special Areas of Conservation (SACs), designated under the Habitats Directive and Special Protection Areas (SPAs), designated under the Birds Directive. Collectively, these are termed European sites and the overall network of European sites is termed Natura 2000. It is an offence under the legislation and regulations to carry out an act which may damage a qualifying species or habitat for which the site is designated. Sunderland has both a SPA and SAC within its boundary and for that reason it has been necessary to undertake a Habitat Regulations Assessment (HRA) to support the SSGA SPD.

The purpose of a HRA is to demonstrate that a plan or project (including planning applications) will not have any adverse effects on the integrity of a European site either directly or indirectly, alone or in combination with other plans or projects. The assessment determines whether the plan/project would adversely affect the integrity of any European site in terms of its conservation objectives. The Planning Authority, as the Competent Authority, can adopt the plan or approve the project only after having ascertained that it will not adversely affect the integrity of a European site.

3.3 National policy

National Planning Policy Framework (NPPF) & Planning Practice Guidance (PPG)

The Coalition Government substantially reformed the planning system through the introduction of the NPPF and PPG, which replace Planning Policy Statements (PPS) and Planning Policy Guidance (PPG). The NPPF is a material consideration in determining planning decisions.



The NPPF, which was updated in 2019 sets out a presumption in favour of sustainable development. Sustainable development is defined as having the following three dimensions:

An economic role – the planning system is to ensure sufficient land of the right type in the right place to allow growth and innovation which supports a strong, responsive and competitive economy.

A social role – support strong, vibrant and healthy communities by creating a high quality built environment, with accessible local services which reflect community needs.

An environmental role – protect and enhance our natural environment, use natural resources prudently, minimise waste and pollution and mitigate and adapt to climate change including moving to a low carbon economy.

Pursuing sustainable development involves seeking improvements in the quality of the built, natural and historic environment, as well as people’s quality of life, including replacing poor design with better design and widening the choice of high quality homes.

It is important to note Paragraph 177 of the NPPF states that the presumption in favour of sustainable development (para 11) does not apply where the plan or project is likely to have a significant effect on a habitats site (either alone or in combination with other plans or projects), unless an appropriate assessment has concluded that the plan or project will not adversely affect the integrity of the habitats site. This is a very clear statement of the overriding importance of ensuring that the integrity of European sites are fully and properly taken into account in development decisions.

Sustainable Development/ Design Guidance

Climate change is recognised as one of the most significant effects facing the 21st century; as such central government has set requirements to combat climate change. Having previously targeted a reduction in carbon dioxide levels of 80% below 1990 levels by 2050, with an interim target of 34% reduction by 2020 (Climate Change Act 2008), on September 30th 2019 the UK Government announced a new target to end its contribution to global warming by 2050. The target will require the UK to bring all greenhouse gas emissions to net zero by 2050, compared with the previous target of at least 80% reduction from 1990 levels.

Half of the country’s carbon dioxide emissions come from the energy used in the construction, occupying and operation of buildings. Therefore sustainable design and construction is of major importance.

The NPPF further supports the transition to a low carbon future through encouraging sustainable development, the use of renewable resources and minimising energy consumption in new developments. Sustainable design and construction seeks to minimise the use of resources, ensure that the built environment mitigates and is resilient to climate change, protect and enhance green infrastructure and biodiversity, ensure sustainable sourcing of materials and minimise waste.

In March 2015 central government removed the Code for Sustainable Homes (CSH) as the national standard for the sustainable design and construction of new homes, replacing it with a set of national housing technical standards and building regulations.

BREEAM

BREEAM is the private sector’s best practice assessment method in the sustainable building design, construction and operation of non-residential development through, measuring a buildings environmental performance. The measures used represent a broad range of environmental and sustainability issues and includes criteria on energy and ecology, enabling developers, designers and building managers to demonstrate the environmental credentials of their buildings.

Building for Life

Building for Life 12 is the industry standard endorsed by Government for well-designed homes and neighbourhoods. The guidance consist of 12 questions which reflect the Government’s vision for housing development in the future and are designed to structure discussion between local communities, planning

authorities and developers. The assessment consists of a 'traffic light system' whereby developments which achieve 9 'greens' are eligible for Building for Life accreditation.

All schemes should aim to achieve 'Building for Life Diamond' status as exemplars giving developers and the council the opportunity to acknowledge and promote good design. Diamond status is available prior to build completion, offering developers the opportunity to market their development using Building for Life.

Manual for Streets

Provides advice for the design of residential streets and represents a strong government commitment to the creation of sustainable and inclusive public spaces.

The prime consideration is that streets should not be designed to solely accommodate the movement of motor vehicles but to also meet the needs of pedestrians and cyclists.

Secured by Design

Is the official UK Police initiative to guide and encourage the adoption of crime prevention measure in new developments through 'designing out crime.'

Criteria and guidance has been produced to achieve secured by design status in several types of new development including new homes and schools focusing on the layout and design of developments including boundary treatments, positioning of roads and footpaths.

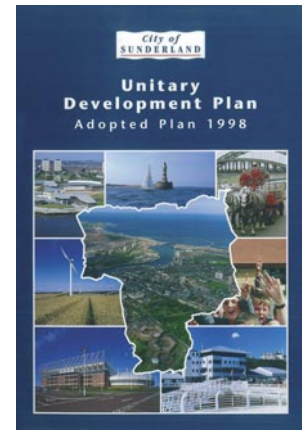
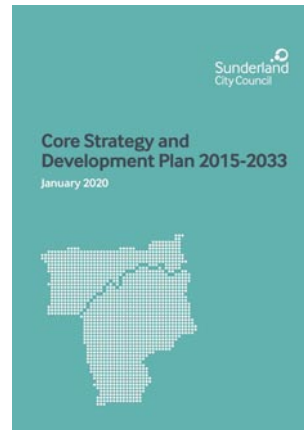
3.4 Local policy

Local Plan and UDP

Sunderland's Local Plan is in three parts sets a clear strategy for the future development of Sunderland. The Core Strategy and Development Plan sets the overall spatial vision and aims for the development of the city to 2033. The Core Strategy and Development Plan was adopted in January 2020. It replaces the majority of policies within the Unitary Development Plan (UDP); as an adopted plan it holds full weight.

The UDP, adopted in 1998, allocated nine sites within the Ryhope/Doxford Park area for residential development (UDP Policy SA9). To guide development in the area the Ryhope Tunstall Periphery Guidance (SPG) was produced and subsequently adopted in 1999 Chapelgarth (UDP Policy SA9.20) and Cherry Knowle (UDP Policy SA9.13, 14, 15, 16) are the only two sites of the original nine that remain unimplemented. Added

to this is the South Ryhope site (UDP Policy SA4) allocated in the UDP for economic development and Land North of Burdon Lane allocated as Settlement Break in the UDP.



The CSDP takes forward the three unimplemented allocated sites within the UDP groups them together along with the settlement break land discussed above, otherwise known as Land North of Burdon Lane and allocates them as a Strategic Site (Policy SS6- South Sunderland Growth Area) to create a new high quality, vibrant and distinctive neighbourhood.

The City Plan

The City Plan is the Council's overarching strategic plan for the period 2019/20 to 2029/30 and will address the key challenges for the city and capitalise on key opportunities to ensure that all residents and communities can benefit from the transformation.

The City Plan, which was adopted by the Council late 2019, will play a key role in supporting the council's and partners' efforts to ensure that Sunderland will be:

- A city where business invests and that benefits from continued regeneration, where people have access to great employment and education opportunities and the city centre boosts economic growth and prosperity;
- A city where people enjoy good health and wellbeing, live happy, independent lives and spend their leisure time; and
- A city where every neighbourhood is safe, clean and welcoming with decent homes and facilities and where every resident can feel proud of where they live.

3.5 Local evidence

Local Plans must be informed by up-to-date evidence bases. As such the CSDP is the product of comprehensive evidence undertaken by the City Council and its partners. Three studies of particular relevance to SSGA include the Settlement Break Review, The Employment Land Review and the Strategic Housing Market Assessment.

Settlement Break Review

For the purposes of the CSDP it was necessary to undertake a review of the Settlement Break allocations within the UDP, to ensure the allocations remained fit for purpose. The Settlement Break Review was subject to a number of updates and public consultation exercises, with the final version produced in 2018.

The review assessed all Settlement Breaks within the city and determined that all of the Land between Silksworth/Doxford Park and Ryhope be removed from the Settlement Break. (see Settlement Break Review 2018), as development would not unduly affect the separation of Doxford Park, Ryhope and Silksworth. However, the review does state that a separation between Silksworth/Doxford Park and Ryhope should be maintained and a north-south corridor should be

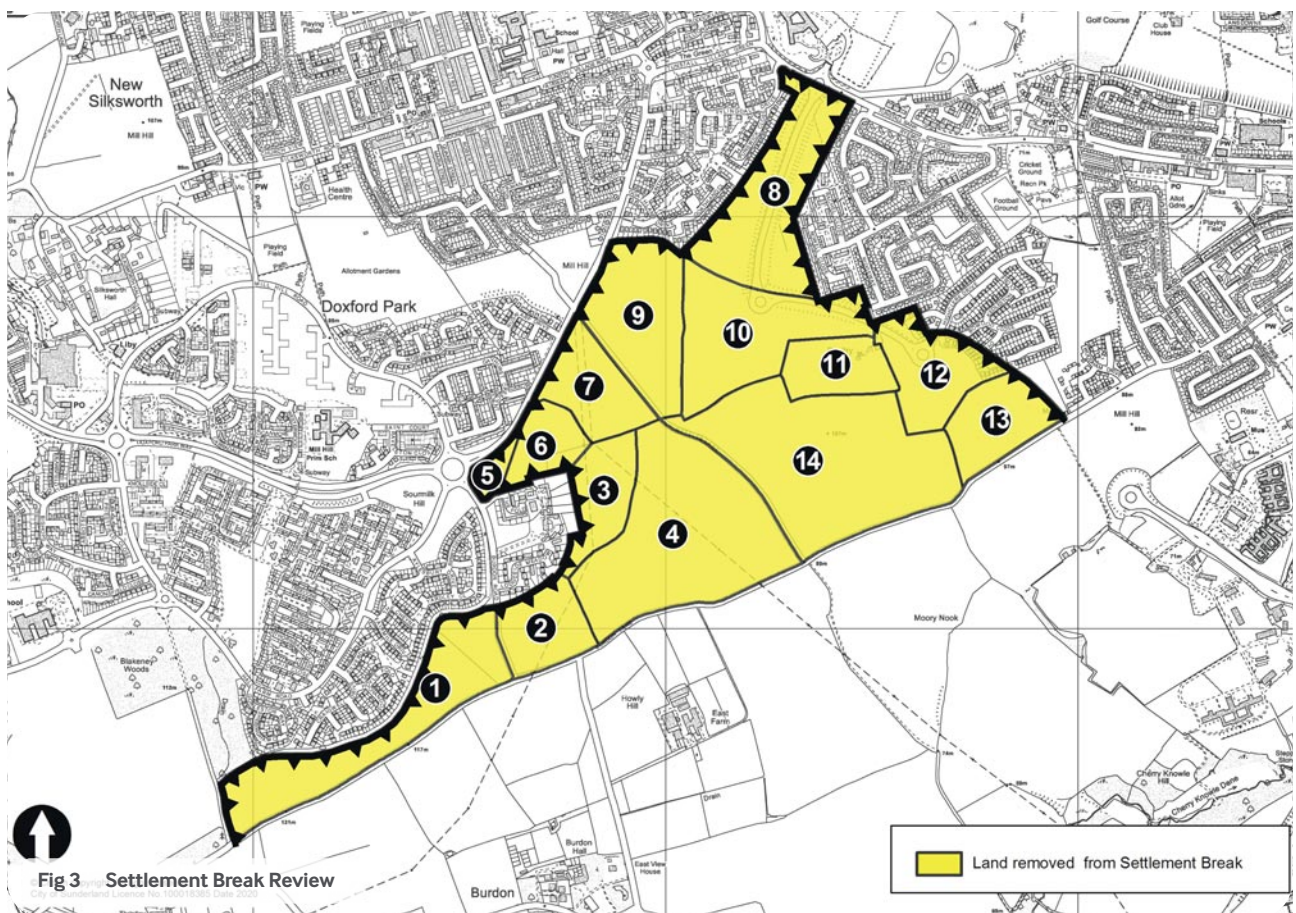
retained linking Tunstall Hills southwards to the Green Belt at Burdon.

It should be noted that as part of the Public Inquiry into the UDP in 1997 the Planning Inspector concluded that the 'unallocated land' between the proposed Green Belt to the south of Sunderland and the edge of the built-up area would provide future decision makers with sufficient flexibility when assessing the need for further development in the area beyond the plan period.

The Settlement Break designation was removed through the adoption of the CSDP.

Employment Land Review

The Employment Land Review undertaken in 2016 highlighted that South Ryhope is a longstanding employment allocation which has remained undeveloped in its entirety. The review recognised the oversupply of employment land in Sunderland South and the fact that there has been no active market for the site as such it concluded that there is no reasonable prospect of the site coming forward for employment uses over the plan period. As a result of this the site gained outline planning permission for residential development in 2017.



Strategic Housing Market Assessment

The 2017 SHMA update identifies a need to increase the number and range of houses to reflect underlying aspirations and to prevent out migration, much of which is due to a lack of appropriate housing choice within the city. The SHMA identified a particular need for more larger family dwellings within the city.

Much like the rest of England, Sunderland has an ageing population the SHMA identifies a need for appropriate accommodation to meet their needs. The council actively encourages alternative designed developments to support the needs of the ageing population.

3.6 Local design guidance

Residential Design Guidance

The Residential Design Guide adopted in 2008 as a SPD (now replaced by the Interim Development Management Guidance) provides advice and guidance on design principles, which should be reflected in any proposed development for the city. Key points from the guidance have informed the SSGA SPD. These include:-

- A thorough understanding of the site context is the starting point for designing a distinct place.
- The success or failure of new developments depends on how well connected they are to existing areas especially in terms of access to local facilities, services and amenities.
- Housing developments should offer a choice of housing and accessible facilities nearby.
- All new residential developments must respond to and where appropriate reinforce the scale, form, character and pattern of townscape which make a positive contribution to the area.
- Residential streets and the public spaces between buildings should include a public realm strategy in their design to make them safe, attractive and usable.
- Detailed design is a key element in the creation of places of enduring quality. The quality of an overall scheme can be spoiled by poor attention to detail. The individual elements of a building e.g. the bricks, windows etc are an integral part of the overall quality of the building.
- Development must demonstrate a commitment to sustainability and energy conservation issues. The reduction of energy demand is the starting point for reducing carbon emissions.

4 Setting the scene

Survey and analysis work has been undertaken to inform the SPD. The work provides a foundation to help shape and form the SPD and inform the quantum's of different land uses that are proposed.

This is only a summary; in order to understand the issues covered in this chapter more comprehensively the documents/evidence listed in the appendices should be considered in full. A full list of evidence used can be found in Appendix 1.

4.1 Natural heritage

SSGA benefits from an array of natural landscape features as well as local wildlife sites, national sites and international sites, connected by wildlife corridors all of which will be considered and guide development proposals in the area.

International Designations

Sunderland contains three sites designated for their international environmental importance:

- Durham Coast Special Area of Conservation (SAC), one of the best examples in the UK of vegetated sea cliffs on Magnesian Limestone exposure
- Northumbria Coast Special Protection Area (SPA) designated for the importance of the rocky shore in supporting Turnstone and Purple Sandpiper
- Northumbria Coast Ramsar site designated for the importance of the rocky shore in supporting Turnstone and Purple Sandpiper.



Purple sandpiper



Turnstone

Parts of these designations run along the coast between Ryhope and Hendon Beach which lie in close proximity to SSGA. The closest part of the proposed growth area is within 400m of the European designation at Ryhope Beach.



Ryhope Beach

National Designations

Two national designations lie in close proximity to the SSGA

- Durham Coast SSSI, designated because of its considerable biological, geological and physiographic interest, in particular its vegetated sea cliff habitat, turnstone and sandpiper (and their rocky shore habitat),
- Tunstall Hills SSSI, designated for its considerable geological and botanical interest, in particular its species rich grassland.



Tunstall Hills



- Blakeney Woods LWS, within the west of the site,

Local designations

At a local level, there are a number of designated Local Wildlife Sites (LWS) within or adjacent to the SSGA, including:



- Ryhope Dene Local Wildlife Site (LWS), lies within and border the site



- Ryhope Denemouth LWS

All of which are primarily designated due to their botanical interest (ancient semi-natural woodland).

- Silksworth Sports Complex proposed LWS

The UDP identifies Local Wildlife Corridors (LWCs) around local designations, (which will be updated within the forthcoming Local Plan). The corridors provide buffer areas through which species may move. LWCs within SSGA include:

- The local corridor associated with Ryhope Dene covers the southern part of Cherry Knowle and the South Ryhope sub-area,
- while that associated with Blakeney Wood covers most of the Chapelgarth sub-area and some of the land west of Burdon Road.



- Cherry Knowle Dene LWS, lies within and border the site

The coastal landscape east of the SSGA is part of a regional wildlife corridor.

Along with the important wildlife habitats in SSGA there are a range of potential protected and notable species in the area, including bats, great crested newts, otter, water vole and badger, as well as reptile records (common lizard and slow worm) in the surrounding area.

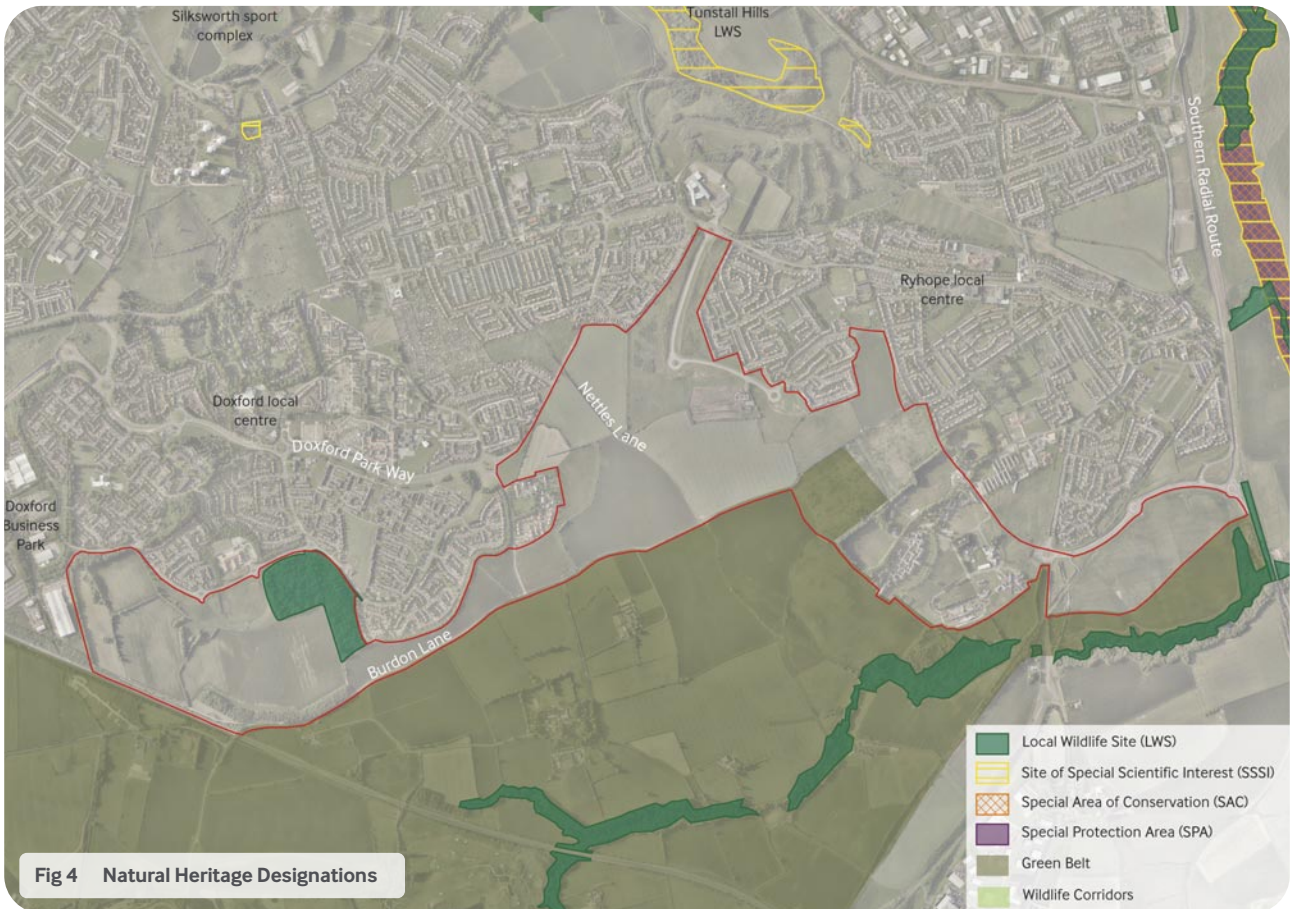


Fig 4 Natural Heritage Designations

TNEI were commissioned in 2013/14 to undertake a Phase 1 Habitat Study of the SSGA in order to map the habitats present in the area, identify any signs indicating the presence of protected species and identify any further species-specific surveys which may be required in order to allow a robust assessment of the proposals to be made. The findings of which can be found at www.sunderland.gov.uk/SSGA

Landscape character

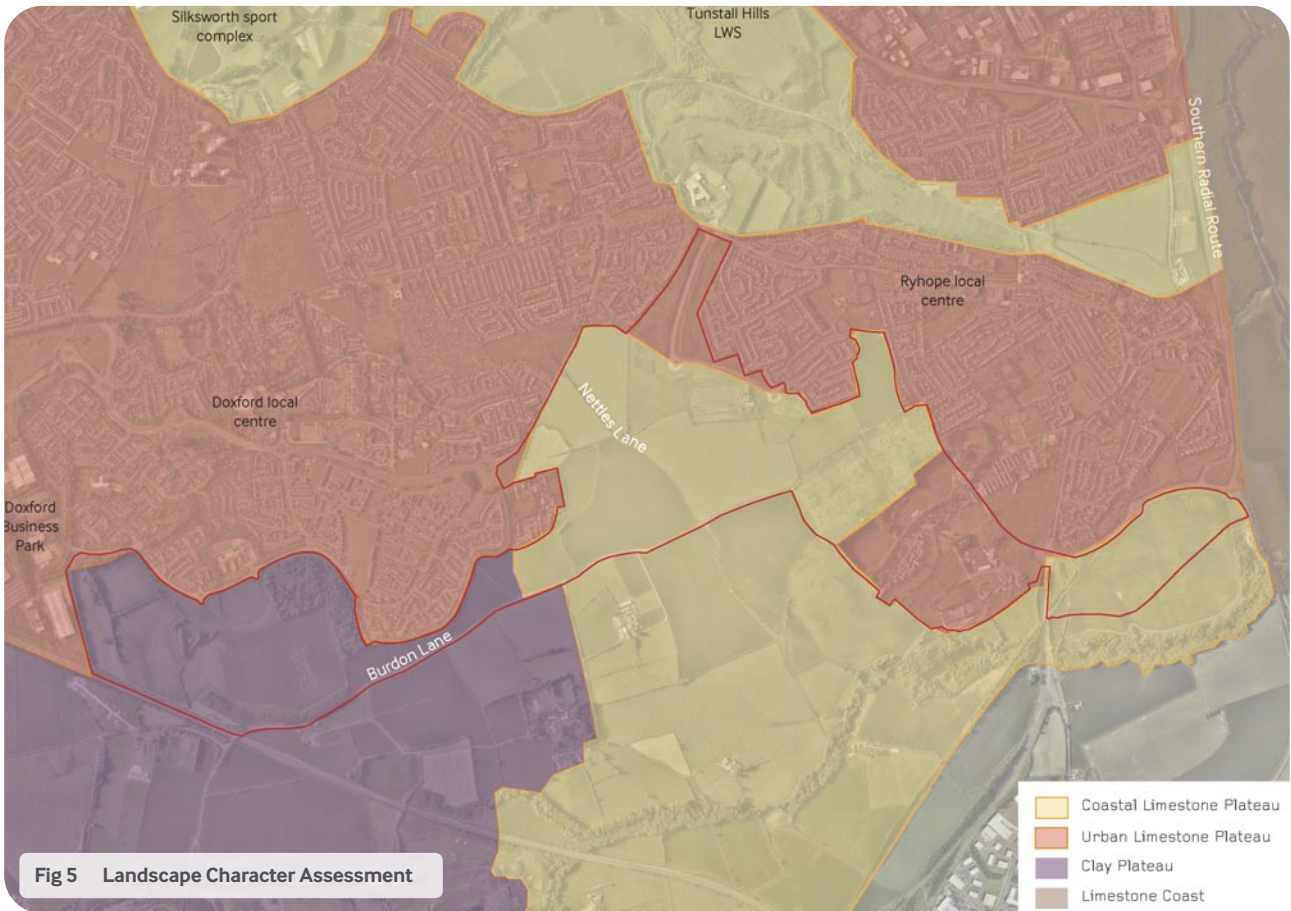
- The SSGA lies within the area classified by Natural England as the Durham Magnesian Limestone Plateau. This area extends along the North Sea coast from South Tyneside to Hartlepool. A unique landscape, this is the only area worldwide where the Magnesian Limestone occurs in a coastal location, creating habitats which are not found elsewhere.
- A City Landscape Character Assessment was undertaken in 2014; the assessment defines a series of Local Landscape Character Types (LCT) and character areas which lie within the Durham Magnesian Limestone Plateau national character area. Broadly these LCTs divide the Limestone Plateau into four linear zones. From west to east these are:

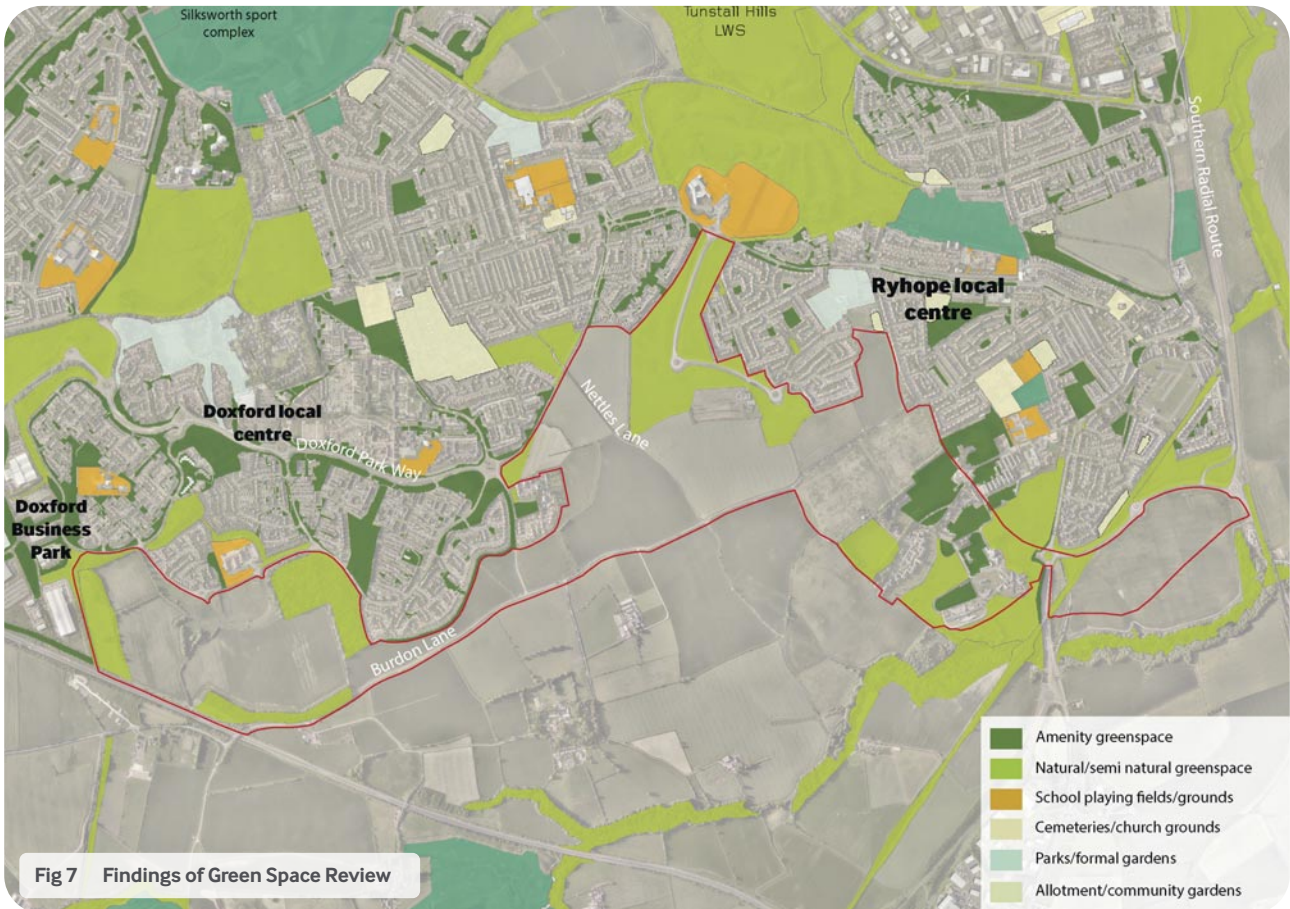
- The steep Limestone Escarpment, which marks the western extent of the limestone landscape and is clearly expressed as a series of hills and limestone outcrops;
- The Clay Plateau, where the limestone is largely obscured by later glacial deposits;
- The Coastal Limestone Plateau, where the limestone is more clearly expressed and the coast has an influence on character; and
- The Limestone Coast, forming the seaward strip of limestone cliffs and the hinterland of coastal grassland.

Overlaid onto these four zones is the Urban Limestone, where development has taken place across the limestone plateau.

Green Infrastructure/Wildlife Corridors

The CSDP identifies a network of indicative strategic Green Infrastructure (GI) Corridors for the city, a number of these intercept the SSGA, see fig 6. GI corridors enable safe and convenient movement for wildlife, walkers and cyclists and can assist the city in meeting several of the council's spatial objectives by improving land for recreation purposes, improving local access and





biodiversity, assist in mitigating against climate change and enable sustainable drainage

Green Space

Green space is vitally important to Sunderland and its residents. It provides people with the opportunity to interact with nature, engage in health activities and promote well-being. Successful neighbourhoods require high quality green space.

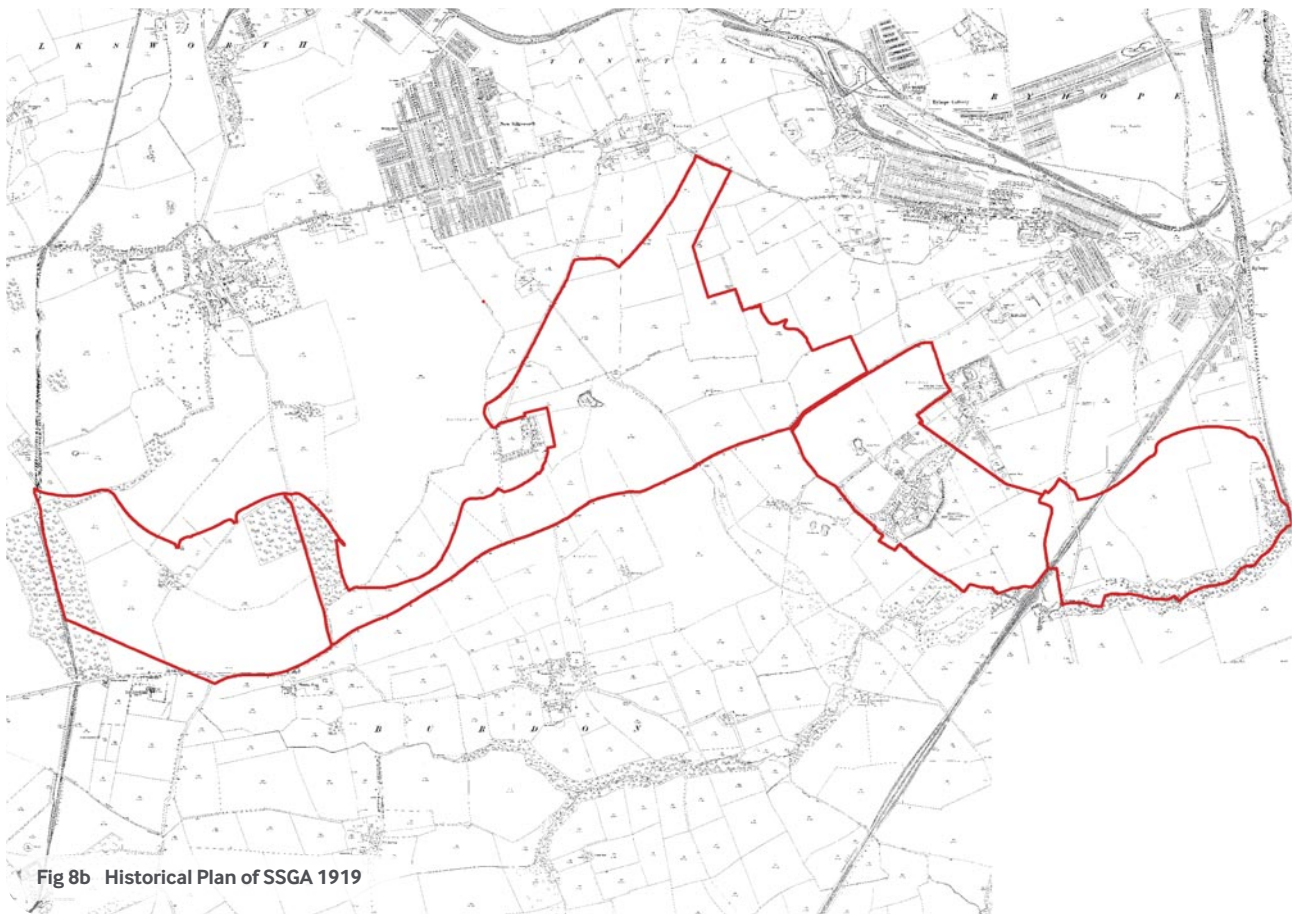
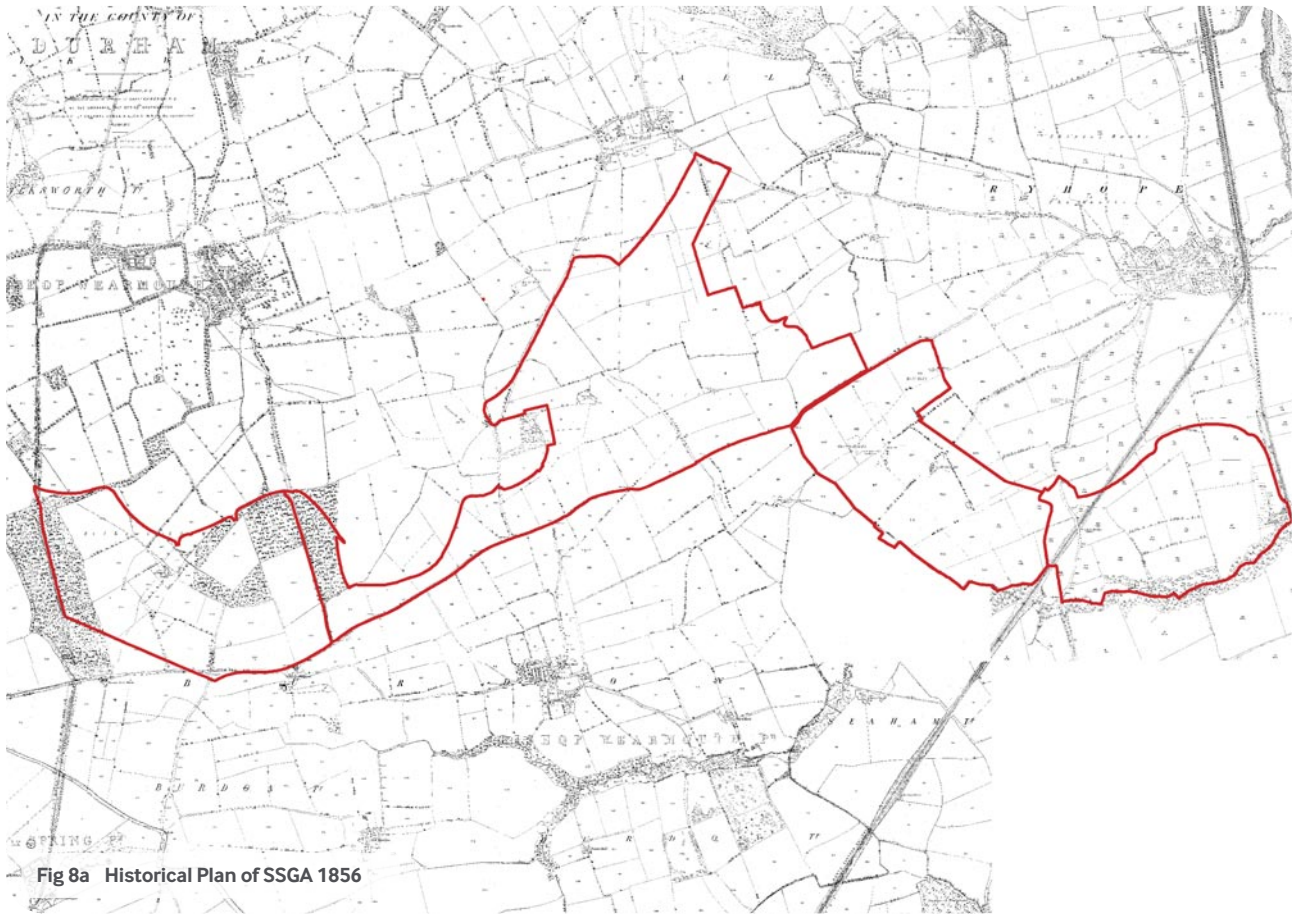
The council completed a updated Greenspace Audit and Report in 2018 to provide an up-to-date assessment of the needs for open space, sports and recreational facilities in Sunderland.



The study concluded that overall the provision of amenity green space, woodland, allotment and community gardens along with natural and semi natural green space across parts of the South Sunderland Growth Area was low. This is in the main due to the majority of the study area being in use as arable farmland and much of the green space that there is, is identified as particularly low quality in terms of recreational use and biodiversity.

Summary

The findings of the various studies undertaken to inform the Natural Heritage chapter will inform and subsequently be reflected in the masterplan for the area. The Natural Environment will be a key feature in the design/layout of the SSGA to ensure the area remains 'unique' and the important species and habitats are conserved and enhanced long into the future.



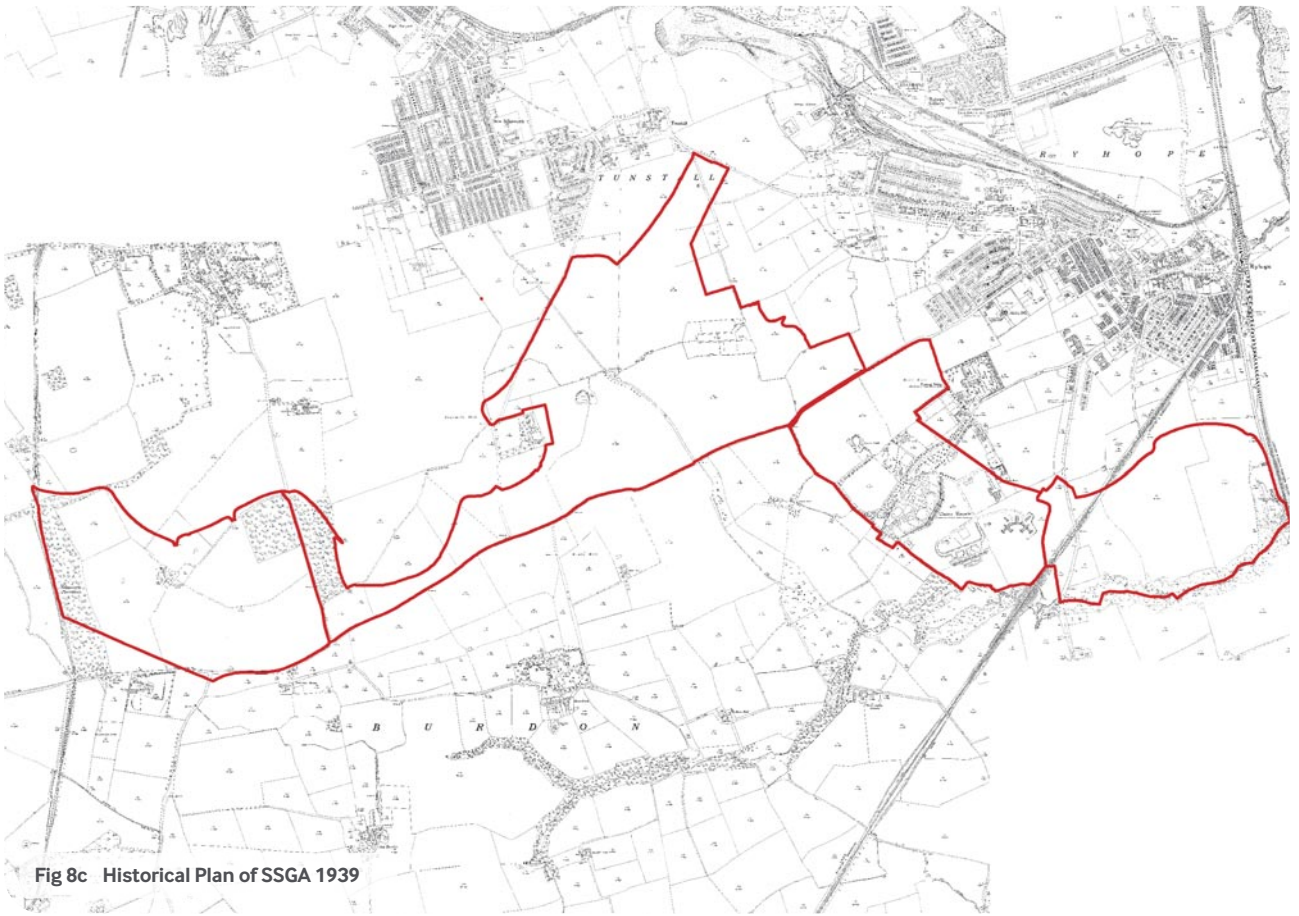


Fig 8c Historical Plan of SSGA 1939



Fig 8d Historical Plan of SSGA 1996

4.2 Built environment

Introduction

Due to the predominantly greenfield nature of the SSGA there is a limited built environment, however the surrounding existing development on the edge of the urban area provides a context for development proposals within the growth area.

Built heritage

Settlement in South Sunderland has medieval origins. The Tyne and Wear Historic Landscape Characterisation highlights 'Medieval' influences on the landscape in the form of areas of open unenclosed landscape and strip fields, evidenced by the survival of ridge and furrow as well as boundary survival. There is evidence of a medieval period settlement core at Burdon and survival of ancient and semi natural woodland at Cherry Knowle Dene.



In the post-medieval period the Historic Landscape Characterisation highlights the area is dominated by field systems which are a mixture of surveyed and piecemeal enclosure of varying dates. By 1856, the majority of the surrounding area was divided by tree belts and woodland in to plots of common grazing land. Nettles Lane and Burdon Lane were already established routes running through the area and a railway line, Seaton Burn, ran to the east. By 1899 the piecemeal enclosure has been re-organised and rationalised with straighter, more consistent field boundaries.

Agriculture remained the main source of employment in South Sunderland until a colliery opened north of Ryhope in 1859. This attracted additional residents to the area and led to the expansion of the village to the west. Ryhope Pumping Station was constructed in 1868 to help meet the demand for water generated by both the new residents and the coal industry. 1893 saw further expansion of the area with the construction of the Sunderland Borough Asylum (later to be renamed Cherry Knowle).

The following years saw more changes. Ryhope Village continued to expand into the surrounding agricultural land. In the 1930's there was further development of land around the asylum. The hospitals site became known as Cherry Knowle when it was taken over by the National Health Service in 1948.

In 1966 the colliery closed, followed by the closure of Ryhope Pumping Station in 1967 Ryhope Pumping Station is Grade II* listed and remains in use today as a working museum.



Ryhope Pumping Station

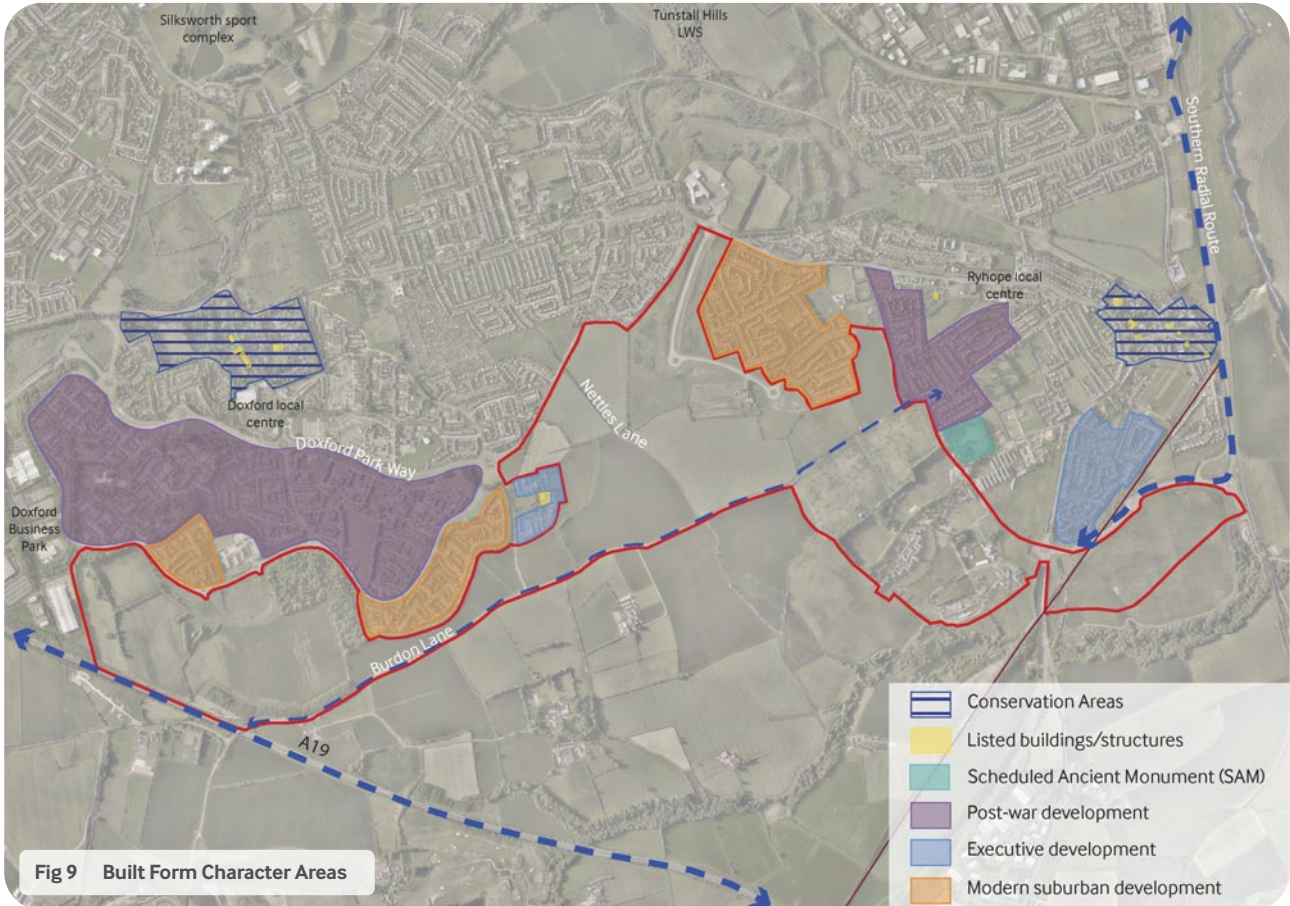
More recent times have seen the development of Doxford International Business Park on the western boundary of the SPD area and residential development at Moorside, Doxford, Rushford, Chapelgarth and in the grounds of Tunstall Lodge. However, most of the SPD area remains in agricultural use with the exception of Cherry Knowle which has been redeveloped to include a hospice and housing.

Built form

The area to the north of SSGA is predominantly suburban in character with the villages/districts of Doxford Park and Ryhope. As can be seen from Fig 9 the surrounding development can be divided into five character areas.

Ryhope Village Conservation Area has an architectural style derived from its origins as a medieval settlement combined with an attractive mix of 18th and 19th century houses.

The Conservation Area, designated in 1971, is a 3-row village centred upon a large triangular village green. The area contains numerous 18th century former farmhouses, barns and cottages, chapels, civic buildings and residential properties and examples of 19th century terraced colliery housing. The Conservation Area's townscape is quite varied, although common building heights of two storeys give a degree of consistency to



the built form. Few buildings rise above the general domestic scale of the village, the most notable being St Paul's Church. Historically, the massing of properties varies between the substantive manor-type 18th century houses of the Wilderness, Chestnuts and Coqueda Hall, to the more refined and quaint 18th century limestone cottages facing the Green.



Due to the built up nature of its surroundings the Conservation Area is not visible from the study area, despite being less than 1 mile from its eastern boundary. Equally there are no views from the Conservation Area in to the study area.

Post-war development, such as the 1970s housing at Moorside and Hall Farm typically comprises a mix of two storey detached and semi-detached properties arranged in regular blocks. Typical features include porches, bay windows and timber cladding. In recent years many of these properties have acquired ground floor extensions. Areas of green space and tree planting are integrated throughout the development giving a green suburban character.

Modern sustainable development such as Chapelgarth Phase 1a and Rushford. These comprise dwellings constructed since the late 1990s. Typically the built form comprises detached, two storey dwellings arranged in irregular blocks. Timber canopies with finials and window surrounds provide architectural interest. A palette of materials including brickwork, timber and cladding is used throughout these developments. Properties are bounded by front and rear gardens. Residential parking is accommodated in integrated garages and formal on-street parking provision is minimal.

Executive Style Housing such as The Fold and Lodgeside Meadow. These typically include large detached 4 to 5 bedroom properties with symmetrical facades and spacious front and rear gardens. Despite their size these properties are closely spaced together. The palette of materials varies through The Fold development and

includes cladding and brickwork, whilst Longside Meadow has a more standardised palette of materials. On larger properties columns and balconies add architectural interest.

Business Park Development at Doxford International Business Park includes a variety of large business and office units surrounded by car parking and landscaping. Scale varies between three and four storeys. Typically units incorporate curved roofs, brickwork, cladding and large areas of glazing.

Listed Buildings

The grade II* listed and Scheduled Ancient Monument Ryhope Pumping Station is located on the edge of Cherry Knowle. It operated as a water pumping station between 1867 and 1967 and is now a volunteer-run museum. The station includes the engine and boiler houses, constructed of bond brick with shale dressing, a Welsh slate roof and stone gable copings and a tall octagonal chimney constructed of brick with an ornamental iron cresting.

A number of listed buildings are concentrated in Ryhope Village Conservation Area which falls outside of the initial SSGA boundary. Listed buildings include:

- Coqueda Hall, a grade II listed building dating from the early 18th century
- St Paul's Church (grade II listed) built 1869-1873

The grade II listed Burdon Hall and Tunstall Lodge, built in 1807, are located off Burdon Road. Tunstall Lodge spent some years as a hotel/restaurant in the late 20th century but is now once again a private dwelling with rendered walls and Welsh slate roof. Sash windows and projecting stone sills provide architectural interest. Burdon Hall, originally a country house and now a private dwelling with offices, is constructed in rendered limestone with a Welsh slate roof. The entrance is located beneath a Greek Doric porch.

Archaeology

As a consequence of the areas history, there are a number of Historic Environment Records (HERs). Further details on HERs can be found in Appendix 2.

Summary

The significance and value of many of the area's surrounding features will strongly influence the choices for SSGA's future. It is important to integrate visual and physical links to these prominent built environment features as part of the structure and experience of the SSGA.

4.3 Views

South Sunderland benefits from an attractive and varied topography. The SPD area rises generally from east to west by a total of 75m. The majority of the SPD area is undulating with the exception of South Ryhope which is relatively flat. Land North of Burdon Lane and Chapelgarth have significant changes in levels to the north. Views outward from the study area include:

- North and north-east – the southern part of the SPD area affords panoramic views towards Sunderland city centre, Tunstall Hills and the North Sea. These views are most impressive from the highest point (135m) in Chapelgarth. However, on lower lying land views are obscured by neighbouring housing.
- East – to the east there are impressive views to the coast. The chimney of the Grade II Listed Ryhope Engine Museum provides a landmark on the skyline, visible from the west of the study area.
- South – to the south there are views to the Green Belt. These views are clearest from the east end of Burdon Lane. Due to tree cover and level differences views to the Green Belt are limited from Chapelgarth and South Ryhope.
- West – due to the increase in levels, westward views out of the SPD area are limited. Views to Moorside from Chapelgarth are obscured by a tree belt and steep change in levels.

Views in to the site from the surrounding area include:

- Views north from the Green Belt – due to the change in levels views in to the SPD area from much of the Green Belt are limited. From Burdon Village Road only the tree belt bordering Chapelgarth is visible.
- Views south from neighbouring housing – there are views in to the SPD area from adjacent housing. In places views are partly obscured by tree belt.
- Views west from the B1287 and Stockton Road – there are clear views in to the low lying South Ryhope site from the B1287 and views in to Cherry Knowle from Stockton Road.
- Views east from the A19 – views from the A19 are obscured by planting.

Summary

Where possible, views should be protected and where viable enhanced. Development should be located and orientated to maximise view points and vistas

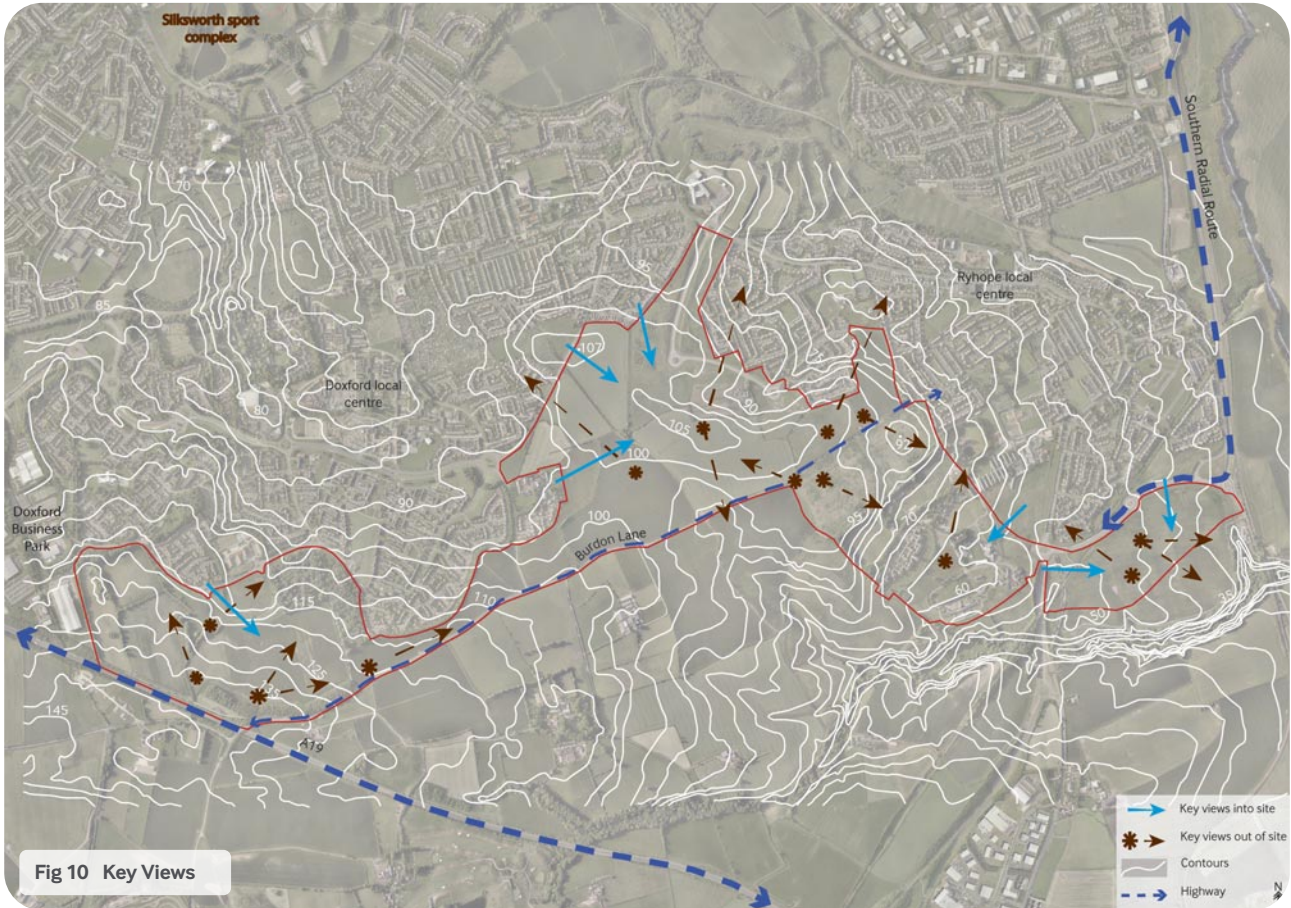


Fig 10 Key Views

4.4 Thriving communities - social infrastructure

The council actively supports the creation and maintenance of sustainable balanced and mixed communities, in facilitating social interaction and creating healthy, inclusive communities

As such, in 2010 the council examined accessibility levels to key facilities on foot and by public transport in order to provide an overview of facility provision across the city. As part of this work a 'walkability index' was produced assessing access on foot to recognised everyday facilities. The index is based on a 500m radius for food stores and a 750m radius for other facilities, which broadly reflects a 10-15 minute walking time for an able-bodied and fit person.

To inform this SPD a review of facilities has been undertaken and an updated walkability index produced for South Sunderland Growth Area.

Retail

The two closest primary shopping centres are located at Ryhope and Doxford. Modern provision at Doxford includes two supermarkets and non-surcharging ATM; the more traditional shopping street at Ryhope includes

three food stores and two non surcharging ATMs. As figure 11 illustrates the majority of SSGA is not within reasonable walking distance of a local food store or non-surcharging ATM.



Health Services

GP practices are located at Ryhope and Silksworth and dentist at Ryhope. Pharmacies are located in the local centres at Ryhope and Doxford. The closest optician to the study area is located at Doxford. Again, much of SSGA is not within walking distance of these health services.

Education

Eleven schools are located in the surrounding area. This includes five primary schools (including four faith schools), two infant schools (Key Stage 1), two junior schools (Key Stage 2) and two secondary schools. There is also one maintained nursery school and existing day-care provision. Five of the primary aged settings are located in Ryhope Village, two at Mill Hill in Doxford and one north of Chapelgarth. However, these primary schools do not have capacity to meet the demand likely to be created as a consequence of development at South Sunderland Growth Area.

The closest secondary schools are Farrington Academy to the north west and Venerable Bede Academy at Ryhope, it is envisaged that with some capital modification these schools will have sufficient capacity to meet the demands of the developments without the requirement for a new secondary school.



Sport and leisure

A Community Wellness Venue is located at Ryhope Community Association which includes a gym and hosts a variety of sporting clubs and classes. Silksworth Community Pool, Tennis and Wellness Centre, approximately 2 miles away from the SSGA is the closest Wellness Centre. Whilst a private gym, David Lloyd Health Club, is housed at Doxford Park.

Sports pitches located in the surrounding area include two performance football pitches (which meet the standard of ground that is acceptable for the Wearside League and/or Northern League) in Ryhope; two bowling greens at Ryhope; a rugby pitch off Ryhope Road; a cricket pitch at Ryhope Recreation Park and Sharpley Golf Course which is located in Seaton to the south west of the study area. The Sunderland Play Pitch Strategy identifies the need for new football pitches, including 3G pitches, a cricket pitch, rugby pitch within South Sunderland, in its widest sense-not just the growth area.



Culture and community

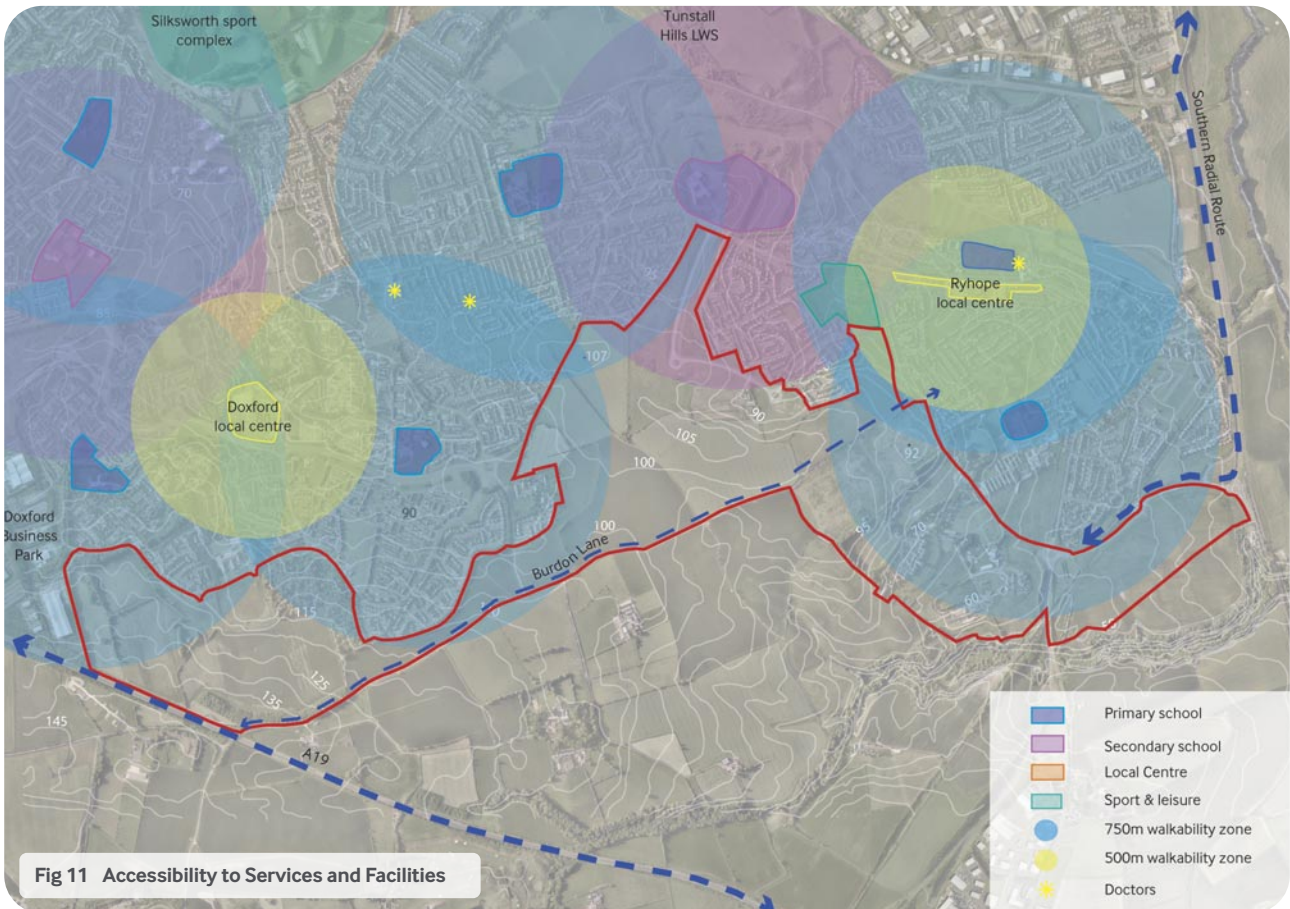
The main cultural attractions are located less than 3 miles away in Sunderland city centre including Sunderland Museums & Winter Gardens, the City Library, the Northern Gallery for Contemporary Art, The Empire Theatre. More locally there is a museum at Ryhope Pumping Station.

Community activities in the SPD area focus around Ryhope Community Association located in Ryhope Village and Doxford Park Community Association located in Doxford's local centre. Blue Watch Youth Project in Ryhope and Box Youth Project in Doxford provide activities for young people in the area.

Three play facilities are located at Ryhope and one North of Burdon Lane. There is a gap in provision of play facilities at Chapelgarth and Moorside.

Summary

As can be see from Fig 11 there are clear gaps in provision with regards SSGA access to services and facilities, these will have to be addressed through development proposals and Section 106s, to ensure that SSGA becomes a truly thriving community in terms of social infrastructure.



4.5 Access and connectivity

SSGA is well connected to national and regional road networks and national and regional cycle routes including the Walney to Wear (W2W) and National Cycle Route 1. It benefits from close links to the A19 and the A1 via the A690 (north & south bound), whilst the Southern Radial Route to the east provides direct access to the city centre where there is rail and metro connections, thus allowing for easy movement locally, regionally and nationally.

Gateways and Arrival

SSGA can be approached from a number of gateways. These gateways differ in quality, with some being more attractive than others. Those that are lacking will need to be addressed through appropriate design solutions.

Vehicular

The SPD area is well connected to the regional and national road network via the A19. The Southern Radial Route (A1018) provides a direct route between Ryhope and Sunderland city centre. Stockton Road connects South Ryhope and Cherry Knowle to the A19 (southbound only; northbound only via Seaton Lane) and the Southern Radial Route. City Way, via Doxford

Park Way, connects Chapelgarth and land to the north of Burdon Lane to the A19 and the A690 linking Sunderland and Durham city centre and to the A1.

In order to understand the potential future impacts of SSGA on the local network and on the adjacent strategic and local roads a transport model has been undertaken. The findings of which can be found in 'SHIM Model Testing and Appraisal of the Local Plan Development Sites Including the South Sunderland Growth Area', Jacobs (August 2014). The impact of the SSGA was also considered as part of the Transport Assessment modelling for the CSDP.

As much of the study area is currently in use as arable farmland the number of roads running through the SPD area are limited, with the exception of completed sections of the Ryhope Doxford Link road, Burdon Road (linking Burdon Lane to Doxford Park Way) and the single-track byway Nettles Lane. Local roads, although rural in character, are relatively heavily trafficked and are becoming increasingly difficult for cyclists, walkers and horse riders - as highlighted in the findings of the transport model.

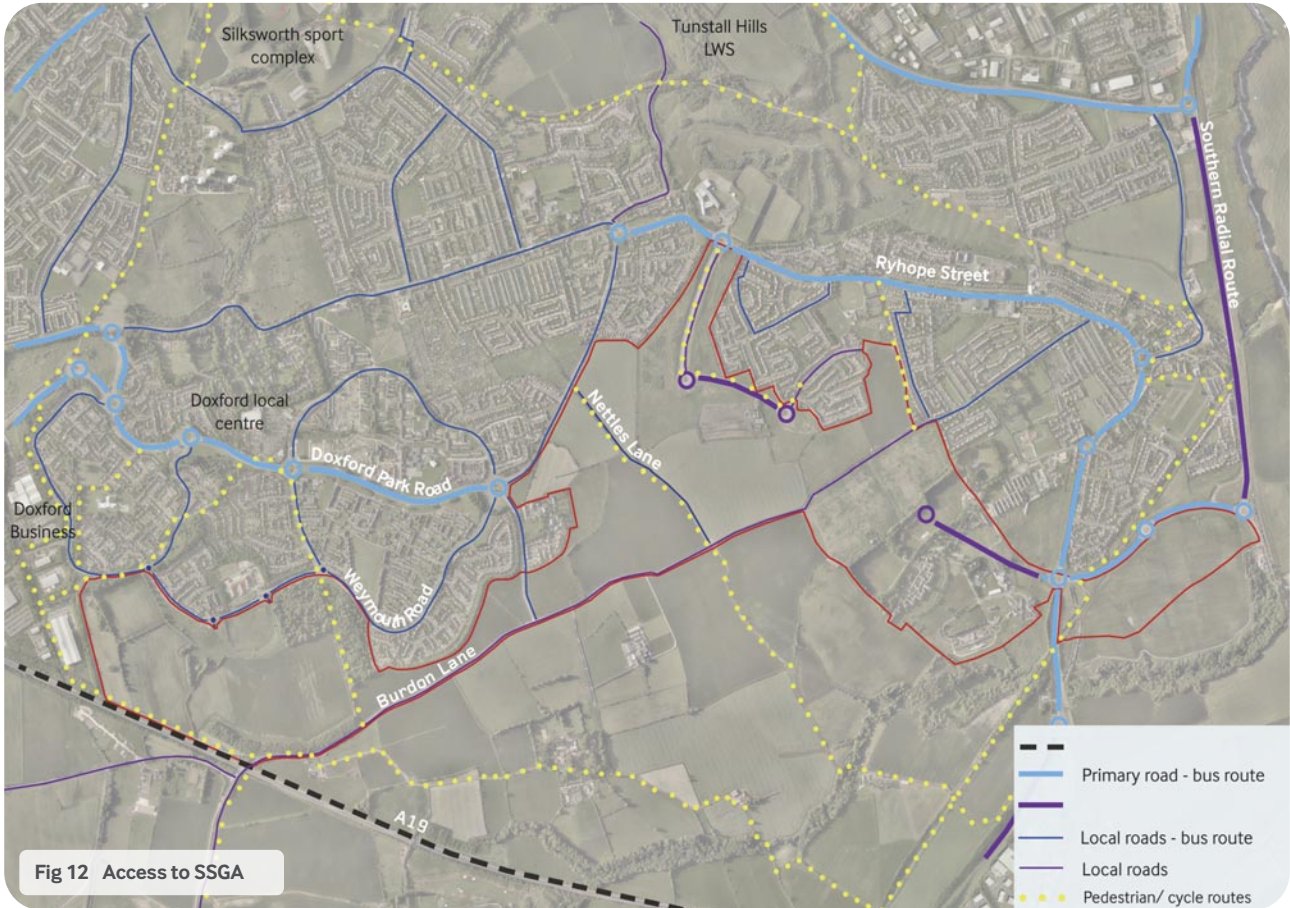


Fig 12 Access to SSGA

Pedestrian – walkability

As the majority of the area is arable farmland public access is limited. There are no public rights of way which cross the SSGA. There are reasonable north-south links within SSGA, but the area lacks East-West routes.

Within the SSGA, both Nettles Lane and the stopped-up road in Blakeney Wood are used as footpaths and informal access is taken across areas of open ground. Beyond the SSGA, there is a bridleway to the south of Burdon Lane, leading to East Cherry Knowle and a public footpath links the B1287 with the coast via Ryhope Dene to the east. The Great North Forest Trail long-distance walking route passes close to the site, making use of a section of Hangman’s Lane to the south. Land at South Ryhope and the majority of the Cherry Knowle site are not accessible to the general public.

Cycle routes

The SSGA is crossed by two national cycle routes. National Cycle Network Route 1, which runs from the North (Shetlands) to the South (Dover) and follows the former railway line from Murton in County Durham through the eastern area of the SSGA, continuing through Ryhope to central Sunderland. The Walney to Wear route (W2W) is a coast-to-coast route linking

Walney Island with Wearmouth. It makes use of the former railway line on the west side of the SSGA to link Hangman’s Lane in the south with Farrington to the north.



There is a lack of dedicated cycle routes running through the study area.

Bus routes

The east of the SPD area is well served by bus; however, links from Chapelgarth and land north of Burdon Lane are currently limited.

Frequent bus services operate from Moorside which is a short walk away. From Burdon Road bus services run to Sunderland Interchange and to Doxford International and Pennywell.

Several regular bus services run along Stockton Road which is located between Ryhope and Cherry Knowle. This includes a bus service between Sunderland Interchange, Dalton Park and Hartlepool and a service between Sunderland Interchange and Peterlee. In addition buses run between Sunderland Interchange and Seaham every 10 minutes (60/61).

Nexus have confirmed that there are currently unacceptable walking distances from the furthest extent of the key sites to nearby bus services.

Metro

Although the Metro service does not extend to the SPD area regular buses run from Stockton Road to Sunderland Interchange where it is possible to connect to the wider Tyne and Wear Metro service via Park Lane station.

The city council continues to work with Nexus with regards expanding and updating the network and also to ensure the alignment of the metro is appropriate and fit for purpose



Summary

The SSGA is already reasonably well accessed however improvements/upgrades and new infrastructure will be required to mitigate the impacts of development proposals to ensure the area is truly accessible and sustainable.

4.6 Sustainability



Sustainability and energy efficiency are high on the national and local agenda as such the council in 2013 commissioned DTA Consulting Engineers LLP to provide a renewable energy study into the opportunities on the use of renewable and low carbon technologies within the SSGA. As such within the SSGA Micro-generation technologies, such as

- Solar water heating, photo-voltaics, biomass boilers and ground/air source heat pump systems should be considered and promoted where viable.

4.7 Utilities

The SSGA is relatively well served by utility infrastructure, with the majority of utility infrastructure is located within the adjacent residential areas along footpaths and roadways.

Mains water and electricity infrastructure, depending on future loads appears to be adequate. Northern Powergrid, the electricity operator, will continue to monitor the capacity. A number of EHV's have been identified at Land North of Burdon Lane and within the Green Belt at Cherry Knowle. These should be accounted for in the design/layout of the scheme.

Natural gas pressure is to a low pressure supply and may need some upgrading.

A regional high pressure gas network runs through South Ryhope development site and should be accounted for in the design/layout of the scheme.

4.8 Other considerations

Geology

The SSGA SPD area is located on the Durham Magnesian Limestone Plateau. In broad terms the geology of the SPD area can be divided into two sub zones – a coastal limestone plateau and a clay plateau. The boundary between the two is taken to be the point to the east of Nettles Lane at which limestone begins to outcrop with greater frequency towards the coast. The Coastal Limestone plateau to the east comprises soft magnesian and shell limestones covered in places by glacial drift of boulder clay, sands and gravels. Soils are heavy, seasonally waterlogged brown stony clay soils with fertile brown earths over deposits of sands and gravels. The clay plateau to the west of Nettles Lane is characterised by soft magnesian limestone covered by thick boulder clay with isolated pockets of sands and gravels. Soils are heavy and can become seasonally waterlogged.

Contamination

The majority of the SSGA has a long history as arable farmland and has not been in industrial use, with the exception of three small former quarry sites. See Appendix 2 for further information. Historic plans

suggest the quarries ceased to operate in the 1800s. There are no records of how these quarries were filled following their use and contamination may be present.

Halliwell Banks contaminated land site is separated from SSGA on its eastern boundary by the B1287. Sunderland City Council designated the site as a Special Site under Part 2A of the Environmental Protection Act and the Environment Agency therefore became the enforcing authority responsible for its remediation. Halliwell Banks does not affect the Growth Area directly; the main risk posed is to the adjacent North Sea and SPA.

Noise Pollution

The A19, Southern Radial Route and Doxford International Business Park have the potential to generate noise pollution; this could have implications for Chapelgarth and South Ryhope and consequently buffer zones maybe required.

Groundwater

All of the growth area lies on the Magnesian Limestone principal aquifer with large areas located within a currently designated groundwater Source Protection Zone (predominantly SPZ 2). The Magnesian Limestone aquifer is a highly sensitive environmental receptor and

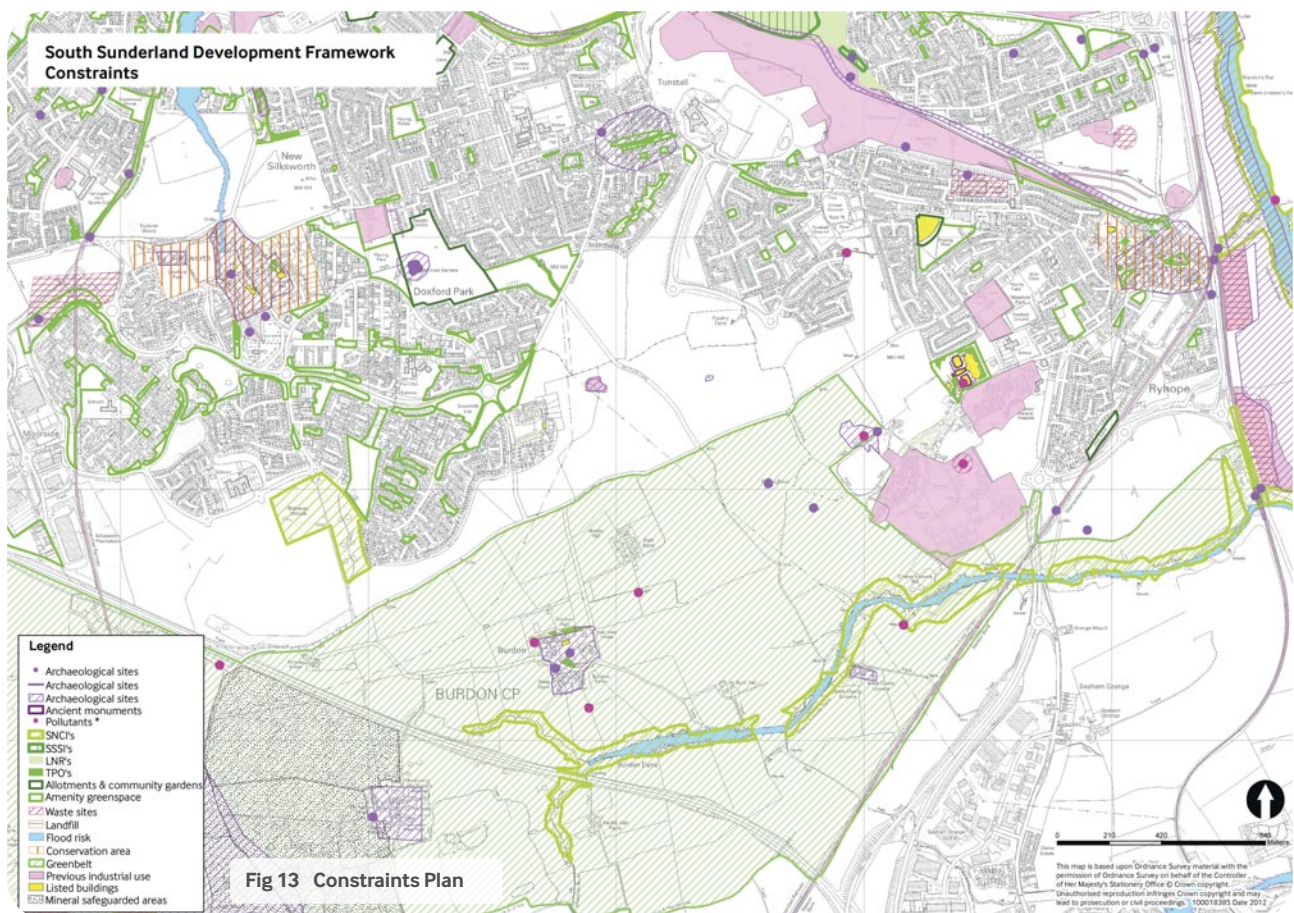
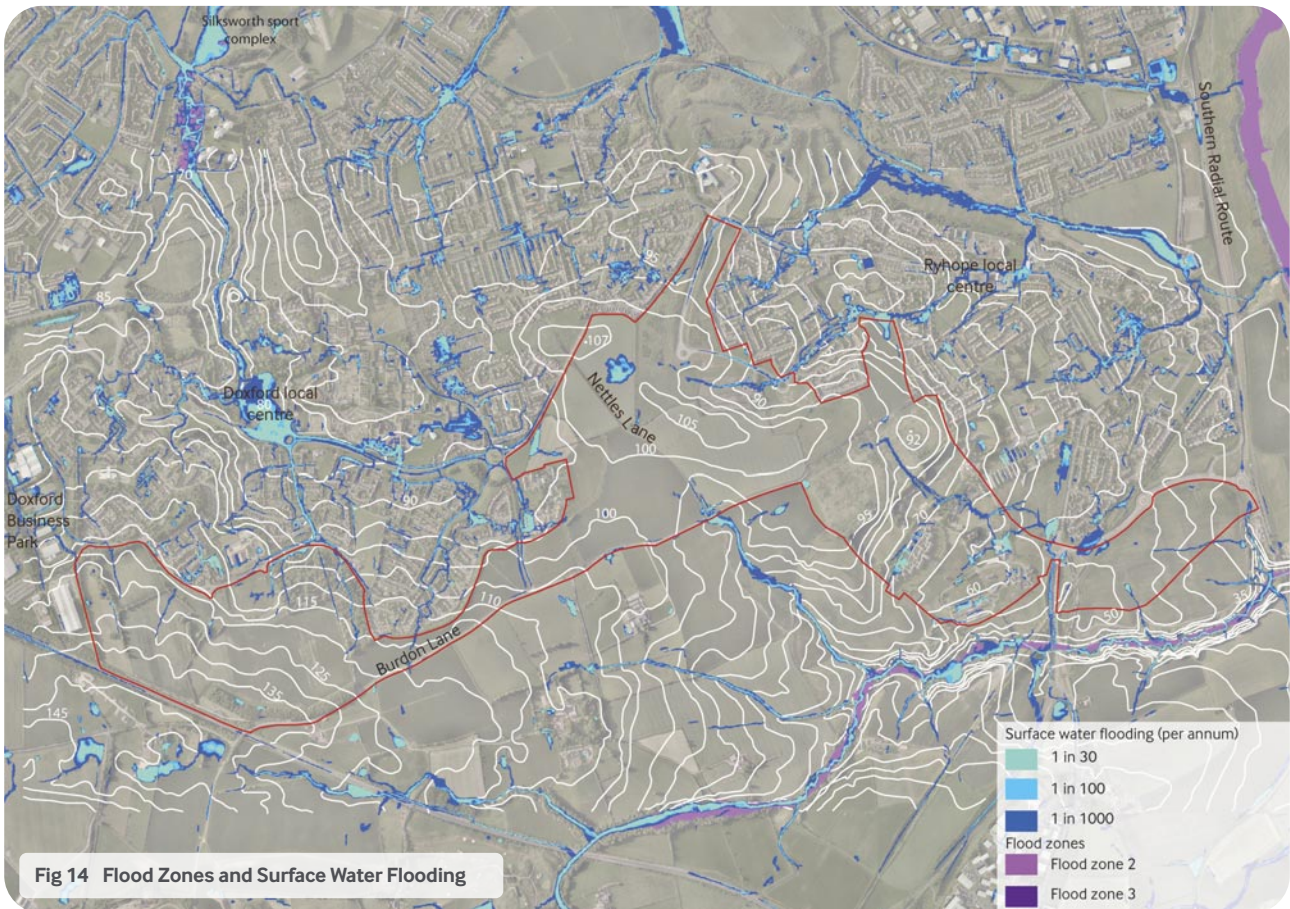


Fig 13 Constraints Plan



is an important source of water for a large number of public, private and industrial supplies.

Water Quality

The Growth Area includes 3 Water Framework Directive river catchments. These are Doxford Park to Hendon Coastal Area, Ryhope Coastal Areas and Burdon to Ryhope Coastal Areas. The catchments main pressures are agricultural and urban runoff. Urban surface water run off presents a diffuse pollution risk which can be managed through SUDs schemes that prevent pollutants reaching water bodies on a case by case basis.

Flood risk and Drainage

The entire area lies in Flood Zone 1 and as such is at the lowest risk of fluvial or tidal flooding. The west of the SPD area falls within a critical drainage area, as identified in the Strategic Flood Risk Assessment. Lower lying land at Burdon Lane and Stockton Road which runs between Cherry Knowle and South Ryhope is susceptible to surface water flooding. An increase in the rate of surface water run off from new development may exacerbate the degree of risk downstream or to the surrounding community.

4.9 Summary of 'Setting the scene'

The SSGA has many impressive features and presents a unique opportunity. Development proposals must aim to maximise the following opportunities and constraints:

Opportunities

- Enhance SSGA's green character and biodiversity, improving wildlife corridors and buffering of Local Wildlife Sites and the Green Belt where appropriate.
- Take influence from buildings in the surrounding area of architectural interest, including the Grade II listed Tunstall Lodge and the Grade II* listed and Scheduled Ancient Monument Ryhope Pumping Station.
- Protect and enhance views to buildings of architectural interest, including the chimney of Ryhope Pumping Station which forms an attractive feature on the skyline.
- Take full advantage of views from the SSGA area across the City the coast and Tunstall Hills.

- Improve the existing provision of community facilities in the surrounding area to meet demand generated from new development.
- Connect development to the network of existing footpaths and cycle routes to encourage the use of sustainable modes of transport.
- Increase demand for public transport provision to the west of the SPD area, to make a new bus route viable.

Constraints

- The topography of the study area allows for impressive views, but may also be a constraint in terms of construction.
- The west of the study area lies within a critical drainage area and there are areas which are susceptible to surface water flooding – development proposals will need to utilise sustainable urban drainage systems and should seek to negate or minimise current flooding.
- The study area falls within a Groundwater Source Protection Zone – development must not have any detrimental effect on groundwater.
- As the majority of the study area is farmland, utilities within the study area are limited with the exception of the regional high pressure gas main which intersects South Ryhope, various overhead power cables and a telecommunications mast located off Burdon Lane.

5 Site analysis

The previous chapter sought to summarise survey and analysis work for the whole of SSGA. This chapter seeks to consider each of the four sites individually and appraise/evaluate each sites strengths, weakness, opportunities and threats in a 'SWOT' analysis. The identification of 'SWOTs' can be used to inform later steps in the master planning process, in seeking to achieve and deliver the vision and objectives for the area

5.1 Chapelgarth



The place

The most western sub-area of the SSGA, Chapelgarth, where the first phase of housing development has commenced to the north-western part of the site, comprises agricultural land, bounded by Doxford International Business Park to the west, the A19 to the south-west, Burdon Lane to the south, Blakeney Wood to the east and the Moorside residential area to the north. The housing to the north is essentially postwar and recent inward facing cul-de-sac development, which plays little or no attention to the wider connections. The settlement interface is characterised by back garden boundaries fencing and a limited landscaped buffer. Key activities nearby include the Doxford local centre, two Primary Schools; Benedict Biscop, Mill Hill and Farrington Sports College school to the north.

Movement and legibility

Two incomplete roundabouts provide potential vehicle access points into the site from Weymouth Road. To the south-west the A19 severs movement, as does the dense mixed shelterbelt which runs along the southern boundary of the site of Burdon Lane. Access is provided along the west and south edges of the area, where the Walney to Wear (W2W) cycle route follows an off-road

route along the boundary. Access to the north onto Moorside Road is poor. Pedestrian and cycle access is also available along the lane within Blakeney Woods, linking Hall Farm Road with Burdon Lane.

Landscape and ecology



The majority of the land is under arable cultivation with a field structure comprising of mature hedges and field boundary trees. These frame the fields, as well as a narrow area of grassland to the west. There are also some distinct clumps of mature trees, including the area around the site of former Silksworth Moor farm in the centre of the sub-area. A mature tree belt runs the southern boundary of the site. Blakeney Woods occupies the north-east corner, comprising ancient semi natural woodland. Topography wise, this is the highest part of the SSGA and as such from various points within the site there are views of Tunstall Hills, Cleadon Hills and the sea.

Strengths, Weaknesses, Opportunities and Threats (SWOT)

Based upon the above site analysis, initial strengths, weaknesses, opportunities and threats have been identified which will be further explored in the Indicative Masterplan, Chapter 8.

Table 1 SWOT analysis of Chapelgarth

Strengths	Weaknesses	Opportunities	Threats
Attractive and expansive northward views over the City to the coast.	Existing development turns its back on the site.	Provide an open and welcoming northern boundary to the development along Weymouth Road, avoiding placement of rear garden fences along this edge.	Lack of appropriate management of LWS.
Mature trees, shelterbelts and woodland provide enclosure, shelter and strong boundary features.	Inadequate connectivity with adjacent areas, despite well-used informal access, e.g. at Moorside Road.	Use existing mature hedges and field boundary trees to provide the framework for new development, maintaining legibility.	Housing development will obscure all views from the cycleway towards the coast.
Well-established native woodland at Blakeney Woods LWS.	Road noise from the A19 along the south-west boundary, despite mature vegetation providing a screen.	Ensure that walking and cycle routes through the new development connect with currently used routes, and with the urban areas to the north.	Increased pressure on LWS from development.
Close proximity to Doxford International Business Park for employment opportunities.	Poor condition of the footpath through Blakeney Woods.	Create additional open space around the south-west of the sub-area to avoid developing the skyline, and to ensure access to the open views from this area.	
		To provide an east-west walking and cycling link through the site.	
		The site should be linked into the Hetton Colliery Railway Cutting.	



5.2 Land North of Burdon Lane



The place

The largest of the four sub-areas extends from Burdon Road in the west, to Ryhope in the east. East of Burdon Road the SSGA broadens northward, taking in the fields either side of Nettles Lane. A new housing development is nearing completion at the eastern end of this sub-area and a further housing development is now under construction to the west of the site. Otherwise, built development within the area is limited to a poultry farm in the northern area, a communications mast beside Burdon Lane and several EHV's - overhead powerlines cross the sub-area. The existing housing to the west and east of Burdon Road is highly introverted and offers very little in the way of positively responding to the surrounding context. Instead the dwellings turn away from the rural fringe, thus having a significant harmful effect on boundaries.

Movement and legibility

To the north of the sub area, a section of Ryhope Doxford Link Road (RDLR) has been completed with two roundabouts which have the potential to provide access for future development. A further section of the RDLR is planned to connect to the B1286. There are no public rights of way which cross the sub area. The single tracked Nettles Lane is a narrow and little-used road cutting across the area and is primarily used as a path. The only public access to the area is a footpath along the eastern edge of the SSGA, to the rear of houses on Bevan Avenue. A bridle path follows Nettles Lane southward from Burdon Lane, towards West Cherry Knowle.

Landscape and ecology



The landscape comprises of medium to large fields, mostly arable with some areas of pastoral grassland. These are divided by hedges with few field boundary trees, creating an open, exposed landscape. Similar hedges follow Nettles Lane. Burdon Road is lined by a metal rail fence to the south and a hedge to the north. Private garden boundaries along the north of the area provide unsubtle boundaries, often comprising a close-boarded fence though with occasional mature trees.

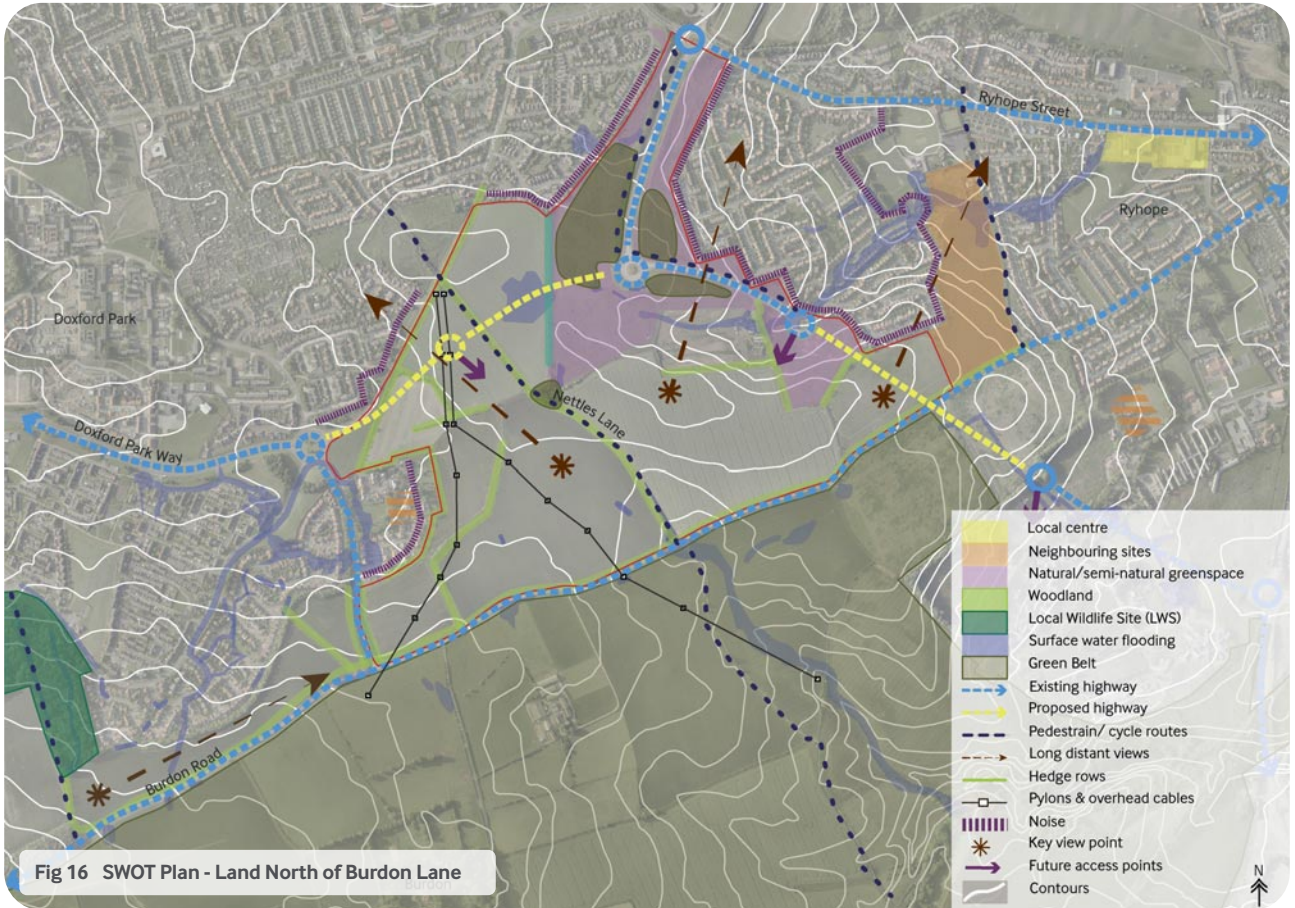
The topography of this sub-area is varied. The southern corner of the sub-area has an elevation of around 125m, the north-eastward slope continues to a low point near Tunstall Lodge, then begins to rise again to a low ridge which runs between the two Mill Hills: from Mill Hill (west) at the north-western corner of the area (107m); to Mill Hill (east) on the south side of Burdon Lane (92m). Thus resulting in open views across to Tunstall Hills and the city centre, with Cleadon Hills in the distance.

Strengths, Weaknesses, Opportunities and Threats (SWOT)

Based upon the above site analysis, initial strengths, weaknesses, opportunities and threats have been identified which will be further explored in the Indicative Masterplan, Chapter 8.

Table 2 SWOT analysis of Land North of Burdon Lane

Strengths	Weaknesses	Opportunities	Threats
Elevated position of the central ridge in relation to the surrounding land, with views in all directions: to the city, the coast and Penshaw Monument.	Over engineered character of new link road access from Tunstall Bank.	Existing poor field boundaries, particularly along Burdon Lane, should be reinforced with new hedge and tree planting.	Avoid developing the highest ground within the sub-area, to retain an open skyline in views from the north, and to retain the wide views available from this area.
Open views from Burdon Lane, particularly when travelling eastward.	Lack of landscape structure, including poor condition of hedges and lack of mature trees, creates an exposed character lacking shelter and human scale.	In line with the Phase 1 Habitat study and GI corridor proposals interconnected greenspace/biodiversity sites should be created throughout the site. These will connect into GI corridors to the north and the remainder of the growth area.	
Green links from Doxford Park and Tunstall Bank, via Nettles Lane, to the countryside south of Burdon Lane.	Weak or overly harsh boundary features, including high rear-garden fences.	Introduce additional woodlands into this area as part of a green network bringing biodiversity and recreational benefits.	
Ecological benefits such as priority species and habitats.	Narrow yet heavily trafficked nature of Burdon Lane limits usability for cyclists and walkers.	Retain some open space as grassland, particularly on high ground in the east of the site where there is more evidence of the underlying limestone geology.	
	Limited formal recreational access into or through the area.	Maintain Nettles Lane as a traffic-free north-south route through the SSGA for recreational users.	
		Topography lends its self to incorporate SUDs into the landscape.	



5.3 Cherry Knowle



The place

This sub-area of the SSGA lies to the south-east of Ryhope and includes the site of the former Cherry Knowle Hospital and its grounds. The site is bounded by Ryhope to the north and east, countryside to the west and Cherry Knowle Dene to the south. A section of the RDLR and hospital facilities of Hopewood Park hospital and St Benedict's Hospice have been developed and work has also started on the first phase of housing development to the south-eastern part of the site.

On the former hospital site, the locations of demolished buildings are still evident within the landscape. Along the eastern boundary the proposed development site is directly adjacent to Ryhope Engines Museum. This is a Scheduled Ancient Monument (SAM) and a grade II* listed building, its grounds also include the grade II listed Cottages and Gatehouse. The Ryhope Engine Museum site is highly significant in heritage and historic landscape terms with immediate and wider landscape key components of its significance; short and longer distance views of the museum are of particular importance as the tower of the pumping station can be seen from miles around.

Movement and legibility

A section of the RDLR is already in place. The road commences at the existing Stockton Road roundabout and heads in a north-westerly direction. The road is dual carriage road with two lanes in each direction, terminating at a new roundabout. To the north-east of the link road the sub-area is now occupied by the newly-built St Benedict's Hospice and Hopewood Park hospital. Due to the former institutional use of the area, there are no footpaths or rights of way across this area, though the National Cycle Network Route 1 follows the disused railway to the immediate south-east, crossing the A1018 on a former railway bridge. This bridge provides the only traffic-free link between this sub-area and South Ryhope

to the east. There is no formal access between this route and the Cherry Knowle sub-area.

Landscape and ecology

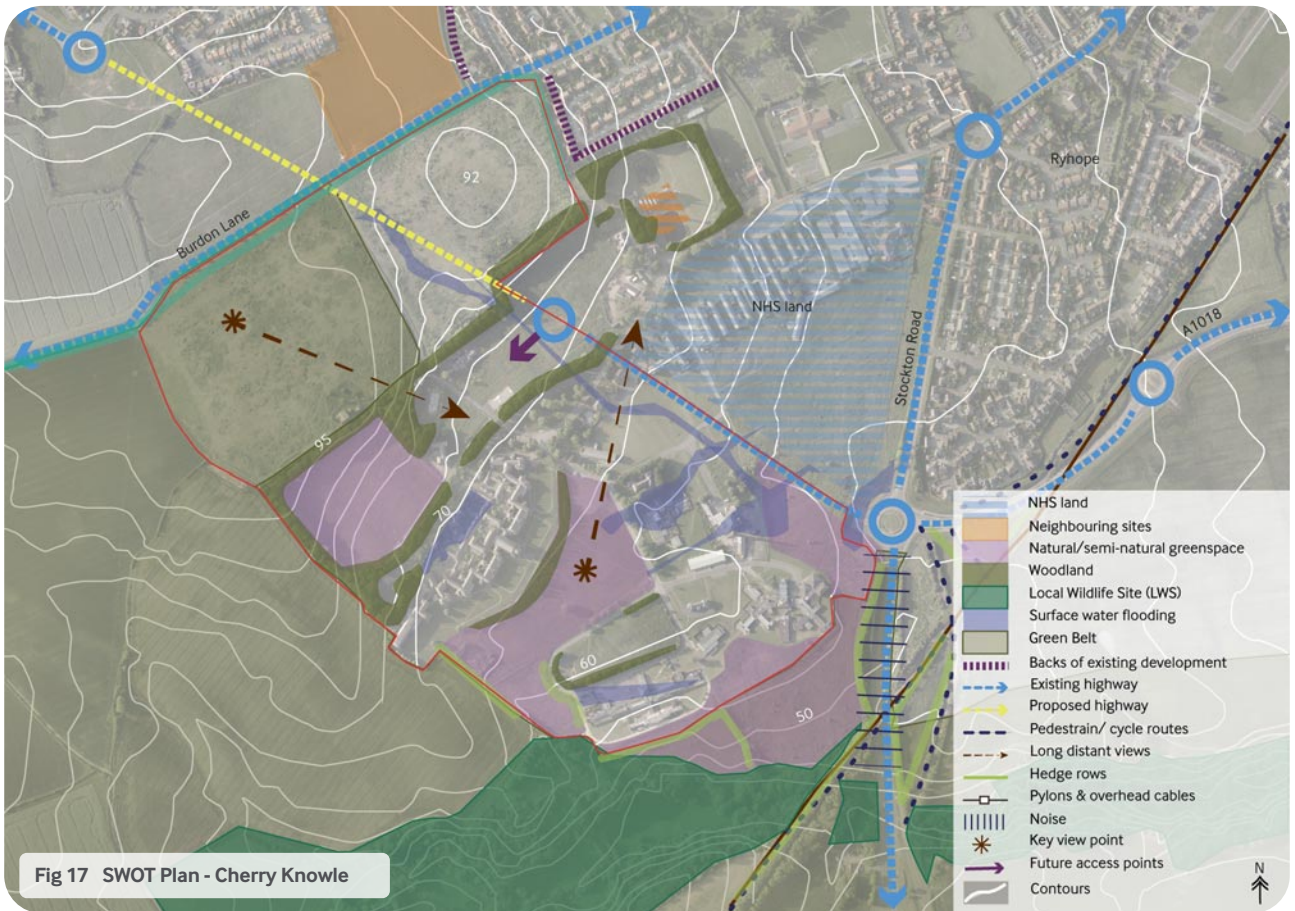
In the north-west, there are two open pastoral fields to the south of Burdon Lane, which are continuous in character with the land north of Burdon Lane. This area, known as Mill Hill, is the highest point of the sub area at 92m and offers significant views both north towards the city and Tunstall Hills and south along the coast. To the south of Mill Hill, a steep slope marks the edge of the hospital site, dropping to an elevation of around 70m in a series of distinct steps. The site incorporates a number of shelter belts, hedges and TPOs. Across the rest of the area the slope is gentler, but shelves gradually south and east to around 50m with seaward views opening out from several more low-lying locations.

Strengths, Weaknesses, Opportunities and Threats (SWOT)

Based upon the above site analysis, initial strengths, weaknesses, opportunities and threats have been identified which will be further explored in the Indicative Masterplan, Chapter 8.

Table 3 SWOT Analysis of Cherry Knowle

Strengths	Weaknesses	Opportunities	Threats
Good highway connectivity with new roads to the north and south of the A19.	The Ryhope Doxford Link Road is a relatively unwelcoming entrance into the area, harshly engineered and lacking planting to soften it.	Create a buffer between development & Cherry Knowle Dene to reduce urban edge effect on the woodland.	Potential contamination due to previous uses.
South facing location maximises views out of the site to wider coastal location.	Informal access into the Cherry Knowle Dene LWS which has caused erosion of the banks.	Carefully integrate the Ryhope Doxford Link Road into the landscape, introducing planting to soften the embankments and fences.	Not being able to deliver sustainable route ways/cycle and pathways that connect with local residential community.
Strong landscape backdrop and setting enables the development of landscape corridors through the site.	Distance from local amenities and separation from existing residential community.	South facing development plateaux, maximises opportunities for passive sunlight.	Increased access to the denes and additional effects from development.
Interesting wildlife present as a result of Magnesian Limestone substrata.	The link road will be continued to the north-west, cutting across the higher ground in the north of the sub-area and potentially resulting in further tree loss.	Work with the sites unique topography to create new public spaces, landscape and habitat creation.	
Close to secluded landscape of Cherry Knowle Dene LWS with its ancient semi natural woodland cover.	Lack of formal recreational access into the area, including severance associated with the A1018.	Develop landscape for visual containment, biodiversity and shelter throughout the site within landscape and wildlife corridors.	
Interesting architectural context, including the listed pumping station and landmark chimney, and high-quality new healthcare buildings and landscape.		Develop views out of the site, utilising the site topography especially towards the coast and historic Grade II Listed Pumping House Museum.	
Panoramic views are available from Mill Hill, looking north, east and south.		Develop leisure routes and cycleways to the surrounding area.	
Hedges, shelter belts and established trees (TPOs).		Develop, strengthen and safeguard green infrastructure.	
		Establish sustainable routes and connections to the local residential community of Ryhope.	
		Draw upon the sites unique magnesian limestone sub strata for landscape design.	



5.4 South Ryhope



This is the eastern-most sub-area of the SSGA and includes the land bounded by the A1018 to the west and north, the B1287 to the east and by the wooded Ryhope Dene to the south and south-east. The sub-area is separated from the rest of the SSGA by the A1018 dual carriageway. The southern boundary of the area is formed by the dense deciduous woodland of Ryhope Dene, the LWS. The eastern boundary is open to the B1287, though the railway embankment forms a barrier to the coast. To the north the A1018, part of the recently completed Southern Radial Route, has little boundary treatment: a young hedge lines the south side while the north is framed by earthworks with rough grassland.

Movement and legibility

National Cycle Network Route 1 follows the disused railway through the area, linking north into Ryhope and south across a bridge over the A1018. The former roadway is identified as an off-road cycle route though it is in poor physical condition. There is no formal access into the rest of the area or along the dene. The dene is clearly used by off-road vehicles and motorcycles, with substantial erosion of dene sides and damage to vegetation.

Landscape and ecology



There is no built development within this part of the SSGA. Almost the whole of this area is occupied by a single large featureless arable field. This area is the lowest-lying part of the SSGA. The denes to the south and south-east are sharply incised across this plateau landform. Wider views are generally limited. From the higher ground to the west there are views across to the sea, though the eastern area is more enclosed. The Ryhope Pumping Station chimney is a constant feature in views west, even from the lowest parts of the site, seen alongside the wooded skyline of Mill Hill.

The lighting columns around the A1018 are a prominent feature in the open landscape and lighting will be a significant presence at night. There is some scrubby woodland along the former railway line to the north. In the west of the site the older section of the A1018 is lined by an embankment and maturing woodland scrub, forming a more solid boundary. Running along the south of the area is Ryhope Dene.

Strengths, Weakness, Opportunities and Threats

Based upon the above site analysis, initial strengths, weaknesses, opportunities and threats have been identified which will be further explored in the Indicative Masterplan, Chapter 8.

Table 4 SWOT analysis of South Ryhope

Strengths	Weaknesses	Opportunities	Threats
Special character of Ryhope Dene LWS, a deeply incised gully lined with ancient woodland forming the southern boundary.	Barriers to access, including the railway line to the east, busy roads to the north and west, and access into Ryhope Dene.	Utilise the site topography to exploit views to Ryhope pumping station and the coast.	Appropriate buffer zones are needed to Ryhope Dene and European Designations on the coast.
Coastal views from much of the sub-area.	General lack of landscape structure except to the south, particularly apparent around the recently completed sections of the A1018.	Improve the visual nature of the SRR through providing a building line and activation onto the highway.	Lighting from the A1018.
Wooded skyline of Mill Hill provides a backdrop to the north-west.	Inappropriate motorcycle access in the dene has caused erosion of the dene sides.		
Immediate access to the National Cycle Network linking to wider recreational networks.	Dominant character of road infrastructure along the A1018 corridor.		
	Underused, undeveloped character of the western part of the sub-area, where fly-tipping is an issue.		

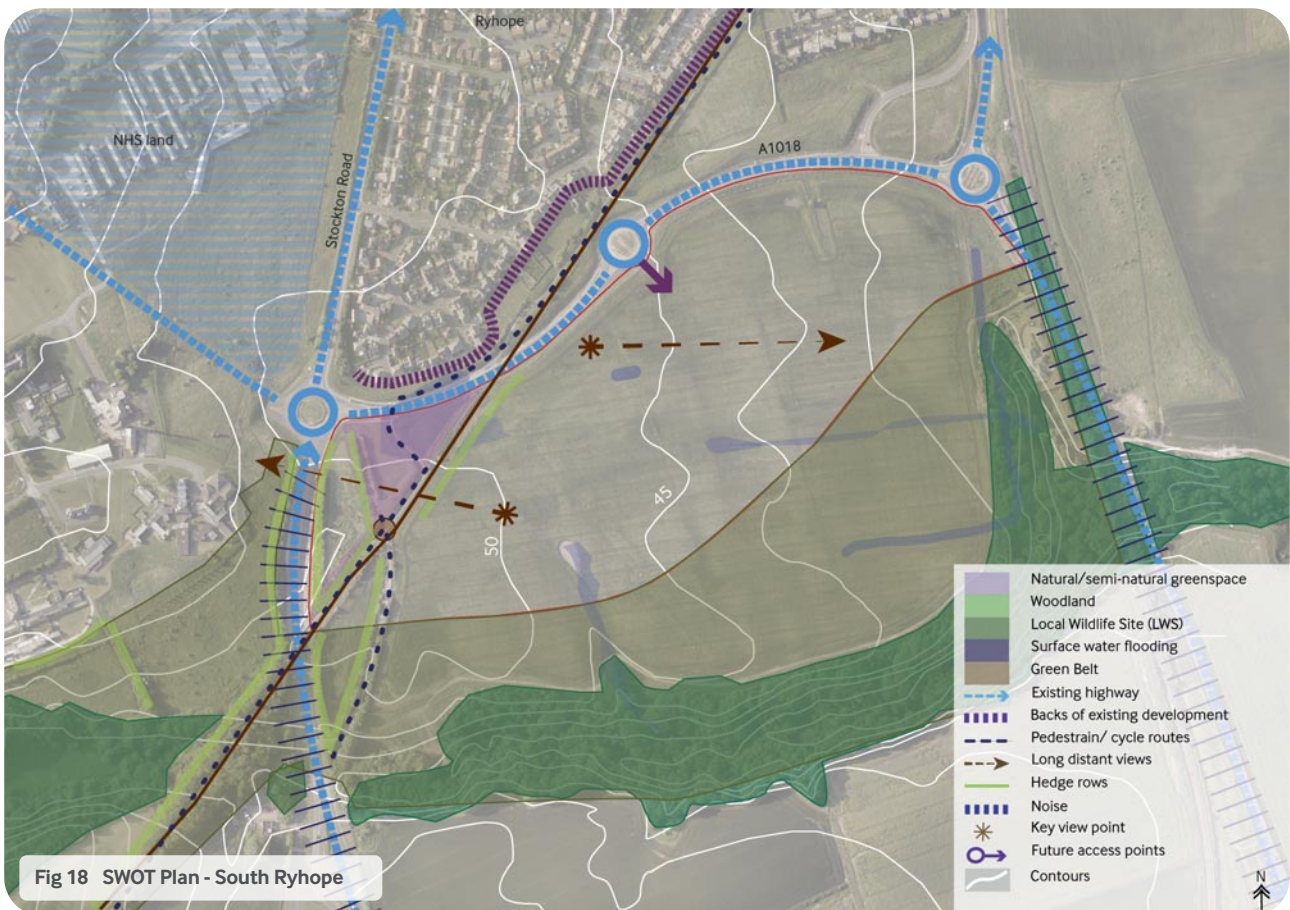


Fig 18 SWOT Plan - South Ryhope

6 Vision and objectives

6.1 Introduction

As highlighted the City continues to see population loss, to address this the council recognises that amongst other things there is a need to diversify the City's housing stock. The development of SSGA, as highlighted in the CSDP, is seen as a crucial element in addressing the current housing imbalance. The area has the opportunity to introduce a much needed mix of housing and provide the City with a supply of larger family homes within an attractive and unique setting.

In order to ensure that SSGA truly meets its potential the following vision and objectives have been established for the area. The vision and objectives have evolved from an analysis and appreciation of the SSGA; its strategic and local context and the supporting studies that provide technical and environmental robustness, thus a viable vision for the SSGA has been established.

6.2 Vision



'A new sustainable community will be created in South Sunderland which provides a choice of high quality homes in a landscaped setting, well connected to the surrounding area and new and existing local facilities. The natural and built environment will enhance the distinctive characteristics of this unique area which borders the Sunderland Green Belt and provides views across the city and coast.'

6.3 Objectives

The following six core objectives aim to help achieve the vision set out above:

- To create a high quality built environment which makes the most of existing topography, landscape features, water courses, wildlife habitats, site orientation and microclimate.
- To create a new community with distinct architectural and landscape features which give the place a unique sense of character.
- To deliver high quality executive housing and wider housing choices.
- Provide new facilities including a neighbourhood centre, local parades, primary school and open space where the greatest number of new and existing residents can access them easily and safely.
- To create development which integrates with the existing community and is well connected to the surrounding area and facilities by road, footpath, cycle route and public transport link.
- To deliver a sustainable community that cares for the city's environment, makes efficient use of natural resources and mitigates against climate change.

The following chapters discuss how these objectives and consequentially the vision will be achieved.

7 SSGA - future provision – principles and parameters

To ensure that SSGA really meets its potential and realises the council and community’s aspirations for the area it is necessary to establish a set of principles and parameters to deliver the visions and objectives for the SSGA. The principles and parameters should be considered and fully reflected through the formulation of detailed proposals/masterplans for the development of the area.

The principles and parameters set out within this chapter are based on and provide a local interpretation of existing policies from the saved UDP polices, adopted CSDP and adopted SPDs.



7.1 Natural heritage

Objective 1

Create a high quality built environment which complements the existing topography, landscape features, water courses, wildlife, site orientation and microclimate.

The setting of new homes is as important as the homes themselves. Prioritising and focusing on the detail of landscape, planting, layout and design are integral to making great places to live. Popular developments are frequently the greenest, leafiest ones with mature trees, well-designed open space with a distinct sense of place.



In satisfying Objective 1, SSGA will be distinguishable by a green and leafy character that makes the most of existing hedgerows, shelter belts and woodlands whilst enhancing the area with additional quality public open space, allotments, meadows, orchards, formal greenspace and wetland areas. Public open space should be multi functional within easy reach of all homes in the area and the existing communities. The extent of greening across the area is in response to the identified landscape and site features that currently exist, as well as constraints such as topography and the need to address the Habitats Directive through the provision of South Sunderland Areas of Additional Natural Greenspaces (SSAANGs).

Key principles

Based on the landscape, ecology, Green Infrastructure (GI), green space and HRA evidence base, the objectives for Natural Heritage are driven by a number of main principles:

- To ensure that new development protects existing priority habitats and species as well as enhancing areas through habitat creation;

- To provide within SSGA a well-ordered hierarchy of green spaces, conveniently located and accessible to all age groups in the community, so that people can enjoy the spaces beyond their immediate house and garden; and in particular, to create pleasant spaces with a clear social function;
- To integrate SSGA within the setting in which it will find itself, to gain maximum benefit from the green surroundings, the landmarks and landscape in the area;
- To exploit this unique location and encourage greater public enjoyment of open space;
- To provide local parks and urban green space within the housing areas themselves;
- To avoid creating areas with a lack of ownership and natural surveillance that result in unclaimed spaces that attract anti-social behaviour; and
- To design, implement and manage green spaces in efficient and innovative ways which encourage and develop a sense of community ownership.

Natural heritage guiding principles and requirements

Habitat Directive

All planning applications will have to satisfy the Habitats Directive, in ensuring the development proposal will not have any adverse effects on the integrity of the Durham Coast SAC, the Northumbria Coast SPA/Ramsar, through the provision of appropriate mitigation measures.

The HRA Screening Report for the SSGA identified that the development of the area, along with sites in close proximity to the SSGA, would undoubtedly result in Likely Significant Effects (LSEs) on the European designations on the coast - the SPA/Ramsar & SAC, particularly as a consequence of human recreational disturbance. As such it is necessary to provide appropriate mitigation measures to counter any LSEs that might result as a consequence of development at SSGA.

There are two key mitigation measures appropriate for addressing the recreational impacts of development on the SPA/Ramsar and SAC:

- South Sunderland Areas of Additional Natural Greenspaces (SSAANGs)
- Suitable Access Management Mitigation (SAMMs) measures at the coast and/or within SSGA

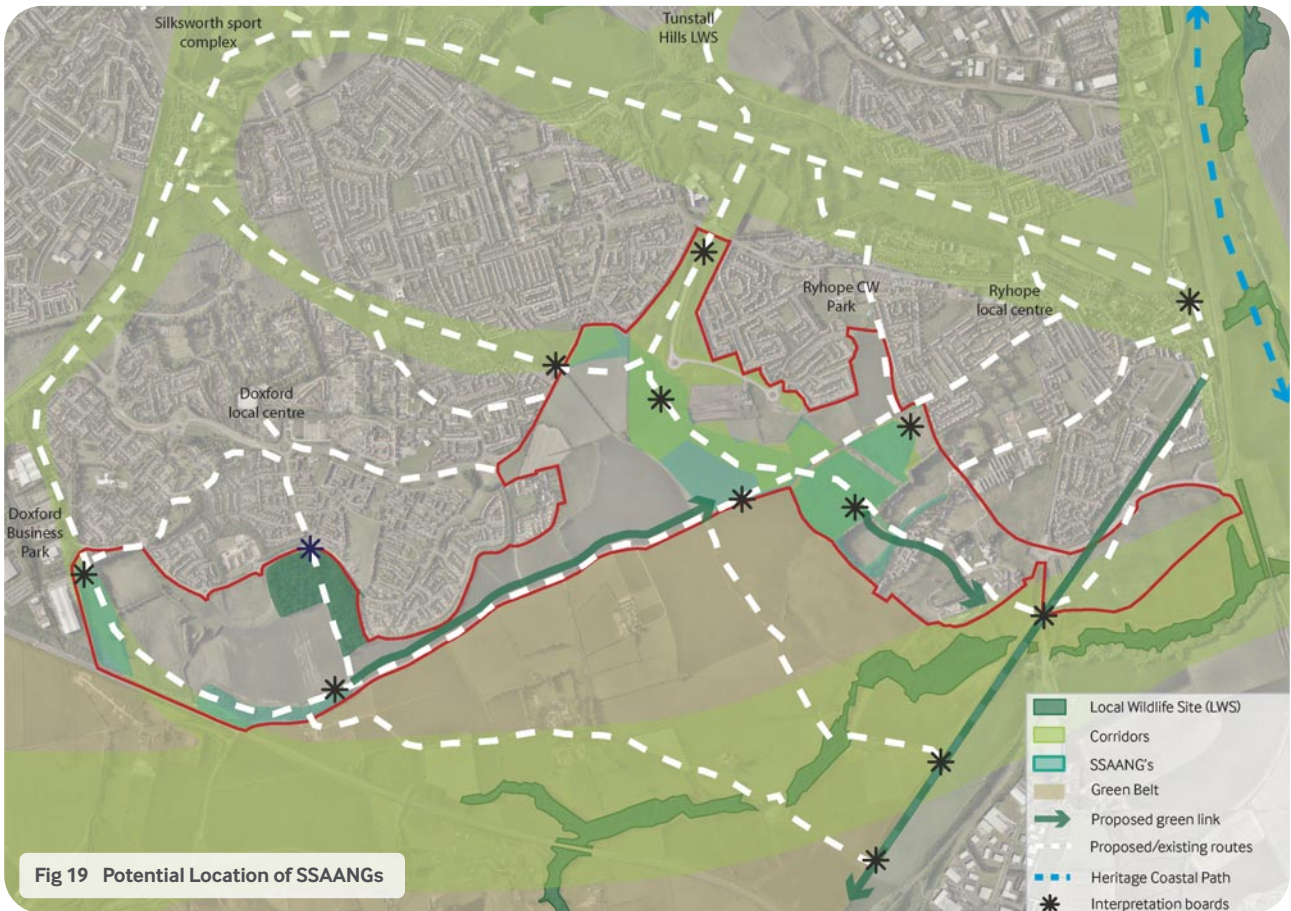


Fig 19 Potential Location of SSAANGs

Due to the scale of development at SSGA, it has been considered necessary to provide a combination of the two measures.

The SSGA SPD HRA provides more context and detail on all matters HRA, including an appropriate monitoring schedule

SSAANGs

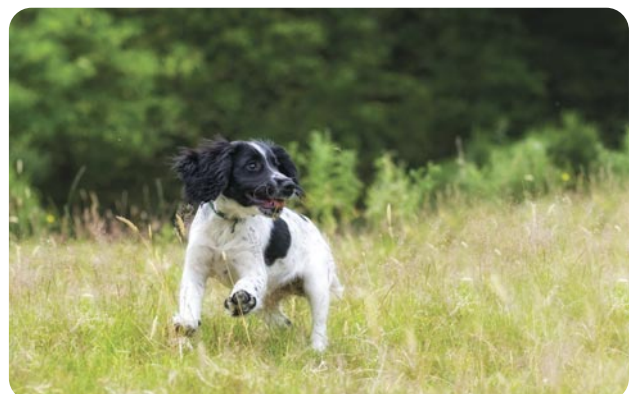
The creation of South Sunderland Areas of Additional Natural Greenspaces (SSAANGs) will be required at a minimum of 8ha per 1000 population or 18ha per 1000 dwellings. SSAANGs should seek to provide areas of alternative natural greenspace to divert people from, in the case of SSGA, the coast. SSAANGs should be implemented by the developer and split over no more than two sites.

Green links should be created to ensure all SSAANGs are well connected to one another, producing a network of accessible SSAANGs.

SSAANGs should be retained and maintained in perpetuity (minimum of 75 years).

Fig 19 identifies the potential location of SSAANGs and green links in the area, including existing GI corridors. This is an indicative plan, the exact shape and positioning of the SSAANG and the precise location of the new green links may be amended during the planning of each SSGA development. The interpretation boards indicated on Figure 19 are indicative and the position and number of boards will be considered through detailed planning applications.

Due to the important role that SSAANGs play in providing alternative naturalised greenspace for dogs to exercise it is crucial that they are fit for purpose to ensure they are attractive to the user. Appendix 3 provides further detail on SSAANG requirements.



The role of SSAANGs at SSGA is to provide alternative green space to divert people away from the nearby SPA and SAC on the coast. SSAANGs are intended to provide mitigation for the potential impact of residential development on the SPA by preventing an increase in visitor pressure on the SPA/SAC.

SAMMs

Contributions towards SAMMs will be requested from all development proposals that come forward within or in close proximity to the SSGA/European designations that cannot accommodate a suitably sized SSAANG, due to the scale of the site.

Notwithstanding this, all development proposals will need to connect to the proposed/implemented SSAANGs.

Off site mitigation measures must be implemented/maintained in perpetuity (minimum of 75 years).

SAMM mitigation requires actions/measures to be put in place in either the location of the European designation to ensure they are resilient to recreational impacts; or on site at SSGA. The council have identified a programme of SAMM measures that include, amongst others, rationalisation of access, information panels and wardens. The full list of SAMM measures are detailed in Appendix 4 and discussed further within the South Sunderland Growth Area HRA.



The table below details how each site is expected to respond to the Habitat Directive in terms of mitigation.

Table 5 HRA Mitigation

SSGA sites	Mitigation
Chapelgarth	SSAANGs- with connections to wider proposed/implemented SSAANGs
Land North of Burdon Lane	SSAANGs- with connections to wider proposed/implemented SSAANGs
Cherry Knowle	SSAANGs- with connections to wider proposed/implemented SSAANGs, including green link between SSAANG and Route 1 Mineral Line green link
South Ryhope	Contribution towards SAMMs. Connections to other SSAANGs/GI corridors. Bespoke boundary treatment to prevent direct access to Ryhope Dene Access Management into the Dene Mouth

Neighbouring sites	Mitigation
Land at Silksworth Lane	Contributions towards SAMMs. Green link to maintain the corridor in this area
Land at Burdon Road/Burdon Lane	Contribution towards SAMMs- with connections to wider proposed/implemented SSAANGs
Land at Rushford Phase 2- Bevan Avenue	Contribution towards SAMMs- with connections to wider proposed/implemented SSAANGs
Any windfall sites within the vicinity of SSGA	Mitigation will be dependent on proposal, size of the site and the scale of development

In line with the Habitats Directive it will be necessary for all planning applications to be submitted along with a HRA Screening Report, produced in conjunction with the Council's SSGA Full HRA/AA.

Landscape

Maintain and enhance the unique character and setting of the SSGA.

The SSGA benefits from an exceptional and unique landscape setting, with a number of existing open spaces and significant tree cover. Maintaining and enhancing these assets to create a high quality open space network will be fundamental to realising the aspiration for an attractive, sustainable development.

Green Infrastructure

Open space within SSGA should comprise of a variety of complementary recreational and ecological functions including:

- Wetland
- Woodland
- Pasture and Meadows
- Retained and enhanced woodlands, shelter belts and hedgerows
- Informal recreation
- Attenuation ponds and other SUDs areas
- Allotments and orchards
- Formal and informal play areas
- New sports pitches
- Enhanced biodiversity/habitat creation

SSGA will comprise significant areas dedicated to landscape, open space, Sustainable Urban Drainage Systems (SUDs) and wildlife habitats commensurate to the scale of development. Green Spaces should be multifunctional; retaining and enhancing the existing valued landscape and ecological features of the area, have informal and formal recreation opportunities and help to create a setting and identity for the new neighbourhood. Public open space should be used to link the various parts of the neighbourhood together, encouraging walking and cycling. The green space within the development should be complementary to and link with the Green Infrastructure area to the south.



All development proposals should be accompanied by an open space, landscape and recreation strategy linked to the Landscape Management Plan

Biodiversity

Existing habitats

Local designated sites should not be negatively impacted upon from future development at SSGA. Designated sites must be enhanced, protected and maintained as a result of development proposals. Suitable buffers between proposals and designated sites will be required to help off set the impact of development and prevent the sites becoming isolated in the landscape.

Designated sites should not be used as public open space.

New habitats

Priority habitats should be created to enhance the area for priority species. New habitats, should, where practical, be located in close proximity to either existing habitats or alongside new areas of habitat to assist in the creation of stepping stones or wildlife corridors which link priority habitats together and aid the movement of local wildlife throughout the area.

Habitat management

Management of all habitats should be sensitively designed and implemented to support and enhance the local wildlife in the area such as farmland birds and pollinators species.

Species

Protected and local priority species must be considered as part of any proposal. Infrastructure and site management must be sensitive to their requirements e.g. impacts of lighting, grassland management.

Existing priority habitats and species found on site must be safeguarded and enhanced, through suitable habitat creation and site management. Measures should be put in place to minimise potential disturbance impacts on species and habitats.



- A network of wildlife corridors linking public open spaces and surrounding countryside should be designed to include and create features such as meadows, wetlands, woodlands and hedgerows.
- The creation and design of SUDs and new wetlands need to complement and enhance species and habitats present in the area.
- No lighting to be installed as part of any development works which would encroach/spill into the designated sites or the buffers supporting them.

Corridors/Buffers

Existing Corridors/Buffers

All existing woodland, hedgerows and other vegetation corridors will be retained and enhanced.

Buffers will be required around each of the corridor types- tree belts (25m), hedgerows(10m), copses and local designations (50-100m), National/European designations (buffer dependent on development proposal).

New Corridors/Buffers

New woodlands, hedgerows and lines of trees should be created to enhance and develop corridors. New corridors/buffers should.

- Be provided where existing landscape edges are thin or non-existent and particularly where existing properties have little or no existing buffer.
- Allow a smooth transition between the development and green belt to the south, particularly along Burdon Road.
- Create structure, where there are large areas of landscape that lack structure i.e. South Ryhope.

Much of the existing character and landscape quality at SSGA comes from the existing trees. Mature trees, hedges and their buffers add considerably to the quality

of residential areas, provide important links for biodiversity, as well as green space and recreation, whilst also providing structure and enclosure for new development.

The size/scale of buffers have been informed by habitat and species presence, the recommendations of TNEI based on good practice and the Natural Environment White Paper by Defra and the Lawton Report Making Space for Nature.

New and existing corridors and their buffers should include recreational routes/paths that link the various parts of the SSGA with the urban areas to the north and with the open countryside to the south. These spaces and routes, which vary in size according to their function, will connect landmark features and provide direct and obvious routes through the area connecting social and community spaces and places, for example primary schools, shops and bus stops. Due to the designations on the coast recreational routes should not be encouraged towards the coast.

Buffer zones should support species-rich grasslands to maximise their value.

Recreation

The recreation and leisure needs of the new community should be met through the provision of multifunctional green spaces and the provision of formal and informal play and sport pitches, to encourage an active lifestyle.

Amenity Green Space

The amount of amenity greenspace required on site will, in part, be determined by the scale of development, the size of the proposed on-site SSAANG, the green links connecting the SSAANG, the SUDs located within the site and the extent of hedgerows, shelter belts, woodlands and associated biodiversity buffers.

Amenity greenspaces should be distributed through the new residential areas; it should provide valuable recreational and amenity space within the heart of a neighbourhood, whilst also providing wildlife habitats in the heart of the community. Spaces should be designed and located to encourage social interaction between all age groups in the community.

These amenity green spaces will provide important local 'greens' and recreational grounds for the neighbourhood therefore they should be of a useable size and have a function, rather than token gestures of verge planting. Their position within the urban form will anchor them to adjacent buildings and facilitate a strong relationship between local resident and park; and as a result will

positively enable and encourage overlooking and “ownership” of the space.

Provision of greenspace should encourage people to use these areas along with SSAANGs in preference to nearby SPAs and SACs on the coast.

Play

Each development site should include a formal/informal play facility. A centralised strategic play facility and a wheeled play area that serves the whole of the SSGA will be accommodated at Land North of Burdon Lane.

It is essential that open space provision within SSGA meets the requirements and aspirations of all residents, including children and young people. Sufficient equipped children’s play spaces must be provided; therefore each of the 4 development sites should include an appropriate play facility, to be implemented by the developer and maintained in perpetuity.

All play facilities should be constructed in accordance with the Council’s Guidance for Play Developments and all play equipment should be of BSEN standards.



Equipped play facilities should be carefully designed within the overall open space network. The location and type of play facilities should be agreed upon in consultation with the council’s sport & leisure officers.

In addition to the 4 play facilities there will be a need for a wheeled play facility also located in close proximity to the new primary school. Development contributions will be sought for the implementation and maintenance of the wheeled play facilities.

Sport

A floodlit 3G play pitch and multi user games area (MUGA) with associated changing rooms will be required.

The sport facilities should be located in close proximity to the new school at Land North of Burdon Lane, to allow for use by the school and the public. Development contributions will be sought for the implementation and maintenance of the sport, recreation and associated facilities.

All sports developments including pavilions, changing rooms, artificial pitches etc will be developed in accordance with ‘Sport England’s Design Guidance’ to ensure they will be fit for purpose.

Allotments

Allotments will be required at 15 plots per 1,000 households.

Allotments should be provided on site and implemented by the developer unless appropriate off-site provision is provided in consultation with the council’s allotment officer.

The sustainability of sites for allotment will need to be confirmed through ground investigation and remediation may be required to make them suitable as allotments.

Allotments should include as a minimum a water supply and internal fencing. It may also be necessary to provide secure perimeter fencing and footpaths depending on their siting. The developer should lay out the initial landscape structure for the allotments; occupiers can then build on it with their own personalised planting schemes.



A management board should be established to ensure the allotments remain fit for purpose and to ensure of their longevity.

Private and semi-private open space

Each dwelling should have access to its own exclusive external space - either as private gardens or as secure communal gardens – so that everyone has access to both private (or semi-private) space at home.

At the lowest level of the open space typology is the private space specific to each home. External space immediately surrounding the buildings must be well defined and enclosed, so that individuals and families feel comfortable and secure. Recommended dimensions and relationships are discussed in the Built Environment Section.

The size of the private/semi- private open space will depend on its orientation: south and west facing spaces should be larger to take most advantage of sunlight.

Management and maintenance of GI and SSAANG

Provide the local authority with a twenty year commuted sum for management and maintenance of GI and SSAANGs

Or

Produce a comprehensive long-term landscape management plan in order to ensure proper management and delivery of different types of green and open spaces on site.

In order to create a truly sustainable development with a robust network of GI, sufficient resources must be allocated to manage the GI to a standard which will encourage increased public use.

SSAANGs must remain in perpetuity to a standard agreed with the council, to satisfy Natural England (NE) and the city council. For this reason the council's preference is for SSAANGs/GI to be implemented by the developer with a commuted sum provided to the city council for its adoption and long term maintenance. In accordance with the council's policy on commuted sums, a commuted sum will be sought for 20 years, after which the council will take responsibility for its maintenance thereafter and the costs associated with its maintenance.

Should the developer prefer to appoint a third party to manage the greenspace, through S106s/conditions/legal agreements the developer must be able to provide Natural England and the city council

with confidence that the green space will be maintained in perpetuity to a standard agreed by the council. A comprehensive landscape management plan will be required. This plan should outline a set of mechanisms in order to ensure proper management and maintenance of open spaces for the future as well as identify the relevant bodies expected to be responsible for the spaces. A management board should be established that includes residents and council representatives.

7.2 Built environment



Objective 2

To create new communities with distinct architectural and landscape features which give the place a unique sense of character.

The City Council is committed to the delivery of communities within SSGA which provide well designed accommodation within easy reach of every day services and facilities, set amidst safe, clean and inclusive public realm and greenspace.

The design of the built environment will be one of the primary elements shaping the quality of SSGA, development proposals should ensure a high quality of design is achieved in the design of buildings, public realm and landscaping, promoting a positive sense of place and creating durable and adaptable environments. Proposals should build upon regional and local design characteristics and include innovative and imaginative solutions to benefit the community and environment.

Key principles

The SSGA stretches from west-east from Chapelgarth, Land North of Burdon Lane and Cherry Knowle through to South Ryhope. Although it will be one community – it is also an area of four distinct neighbourhoods, each with their own character, development history and prospects. There are a number of core components that will contribute to the creation of the SSGA new communities:

- The design of new areas for SSGA should be conceived as a whole rather than as a collection of housing estates, integrating with existing communities.
- Dwellings, shops and public institutions should be located in close proximity to each other.

- The relationship between built form and the public realm and the character of the landscape, should generate a lively distinctive environment.
- The high quality design of buildings and spaces will positively contribute to the vibrancy of the SSGA neighbourhoods and help the community to maintain a sense of pride and ownership.
- There should be careful consideration of the relationship between street type, block form and the treatment of the public realm.

Built environment guiding principles and requirements

Place-making

Development proposals within the SSGA will be required to deliver schemes that achieve high quality standards of design, following the principles of Building for Life 12 and Secured by Design, whilst respecting the surrounding built, landscape and archaeological features within and neighbouring the site.

Building for Life 12 and Secured by Design are nationally recognised design tools which promote the construction of high quality, well connected and safe communities.

Building for Life 12 guides the better planning of new development through urban design standards which create safe environments that provide everything that should be expected within a new community. Development proposals should seek to achieve building for life accreditation, therefore providing a mark of a good place to live.

A fundamental reliance on standard elements or layout in a design scheme is not acceptable, in the interest of creating a distinctive place:

- Design elements should demonstrate tailored responses to context, to the exact location and the public realm in order to create an engaging built environment.
- Elements should offer flexibility, an inherent high quality of design and sense of positive identity for inhabitants.
- All proposal will be expected to support basic streetscape design and contribute positively to the character and identity of the area, especially in relation to human scale.

Development layout within SSGA

Proposals will be required to:

- Form a perimeter block development which makes clear distinctions between public and private space.
- Provide a variety of block sizes.
- Arranging buildings so that they enclose and overlook streets and spaces and have a positive and direct relationship with the public realm.
- All developments should front onto the public realm, natural features of the landscape, pedestrian/multi-user routes, open space and ecological buffers.

Many of the best residential environments display a clearly defined and coherent structure. This is characterised by a framework of interconnected routes which define 'blocks' of housing.

Providing the correct development structure is vital to creating successful neighbourhoods within the SSGA. The key priorities in determining the layout of proposals is to provide a legible block structure which clearly defines a public front and a secure private back. Sites must be designed so that their block and movement network accommodates these principles. The most appropriate form of development will therefore be a 'perimeter block'.

A perimeter block style development allows for a flexible form which can accommodate a range of buildings types and densities. The style of development will also ensure maximum natural surveillance and activation onto the public realm through buildings fronting onto a public street whilst also ensuring security to the rear of properties.



Fig 20 Example of perimeter block development

To ensure the SSGA provides a safe and welcoming environment which relates well to the natural features of the site all development blocks should be designed to face onto public realm, natural features of the landscape, pedestrian/multi-user routes, open space and ecological buffers. Proposals which provide rear gardens or blank/inactive elevations onto the above elements will not be considered acceptable.

Heights, density and form

- New dwellings will be predominately two storeys with elements of two and a half storey properties within appropriate locations. Three storey properties will only be included as landmark buildings or in gateway locations.
- Highest density of 35dph will be provided around the neighbourhood centre and in areas which border the existing residential areas.
- Low density developments of larger family homes between 15-20dph and, where suitable, 10-15 dph will be provided throughout the SSGA. The lowest densities should be located in areas bordering SSAANGs and the rural edge of the SSGA.

Development at SSGA should include changes in building heights, density and forms. This will ensure that there is character and visual interest and will also enable the layout and detailed design of the proposals to respond to its context.

A range of dwelling densities will be provided across SSGA. Densities should relate to factors including edge of settlement location, the character of the particular area, the relevant street type and the underlying landform characteristics. Developments located next to neighbourhood centres will be expected to be of a higher density than development which fronts onto landscape features or local park land/SSAANGs.

In general the relative higher densities (30-35dph) will be defined by the use of more semi-detached/linked dwellings, narrower plan forms and reduced frontages, with more 2.5 storey properties. 3 storey properties will only be considered appropriate on key gateway locations and in areas with high levels of activity, such as the neighbourhood centre.

The lower density housing will be characterised by detached and semi-detached properties, with deeper frontages (or "set backs") and a 'greening' of the area through the incorporation of trees, substantial front gardens, hedges and open space.



The range of building heights, densities and form will need to be fully explored and defined as the scheme design advances. These factors will be important considerations in defining the character areas to be established across the site.

Character

SSGA will have a unique character which distinguishes itself from the surrounding built environment.

Create distinctive character areas within the four subareas which respond to the local context and natural features of the site but remains co-ordinated with the overall character of the SSGA.

Due to the greenfield nature and lack of existing built form the development of the growth area provides an ideal opportunity to create its own unique character that distinguishes itself from the surrounding built environment.

As highlighted, SSGA is not just one community; it is an area of four distinct neighbourhoods, each with their own character and development/natural history. This should be evident in the design and character of proposals for development sites. Although variations in character are encouraged and expected within the site there should be a harmony between the four sub areas.

The character of the developments and therefore the local neighbourhoods, created within the SSGA will primarily be defined by the sites location, context and the variety of built form and landscape features within it. The appearance and character of the development will not just be about the design and architecture of individual buildings but also about how the streets, plots, public realm, landscape and natural features are all designed and interact with one another.

Through the creation of distinctive character areas within the four sub areas which respond sensitively to the site and its setting, developments will increase the

legibility throughout the SSGA whilst also creating a neighbourhood with a sense of ownership and value.

Proposals which do not respect the natural and built context of the site or create a locally distinctive character will not be considered acceptable.

Architectural design

Proposals for the SSGA will:

- Produce high quality developments which are designed to create neighbourhoods that are distinctive to the SSGA yet responsive to their surroundings.
- Establish high quality landmark buildings at key locations including gateways and view termination points.
- Include appropriately designed corner turning properties where elevations which are seen from the street have adequate fenestration and detailing.
- Provide activation and natural surveillance onto the public realm, natural features and streetscene through including active ground floor uses and the distribution of windows and doorways.
- Proposals which feature blank and inactive elevations which front onto areas of the public realm or areas with activity will not be considered acceptable by the council, as will those which offer a poor or weak relationship with the streetscene.
- Sensitively integrate developments with the landscape and existing vegetation.

Whilst the layout and massing of developments throughout the SSGA is important, of equal importance is the detailing of the built form.

Emphasis will be placed on providing new high quality homes and building designs that are enduring, desirable and attractive. Simplicity of form and a good sense of scale and proportion, coupled with the use of quality materials will underpin designs.

Elevational treatment including façade proportions, window size/positioning and the materials used play a key role in how the SSGA will appear now and in the future and how developments will interact with their surrounding neighbourhoods.

The use of perimeter block developments will create instances of corner properties which face onto the public realm, it is essential that these properties are designed to provide an appropriate level of activation and detailing onto the streetscene.

Not only will this increase natural surveillance but also maintain a continuity of frontage. The inclusion of a house type with dual aspect elevations would provide an adequate response.



The inclusion of landmark buildings within development proposals will improve the legibility throughout the SSGA by establishing focal points and aiding in view termination points.

Providing a variety of house types with a continuous building line will encourage interest and establish character within the subareas.

House types can provide narrow or wide frontages which can be arranged in detached, semi-detached and linked forms. Depending on which type is used it will have an important impact on the layout, quality and identity for a development including;

- the type of parking provided
- the quality of housing frontage and setback, in terms of the extent to which the front garden/defensible space is dominated by hard standing and or cars and the relationship between the property and the street
- continuity of built form along the street and enclosure.

Further details on architectural design are provided within the Masterplan and Design Code in Chapters 9-10.

Boundary Treatment

The role of the different boundaries, within the development varies between front, rear and side boundaries. Boundary treatments within proposals must:

- Clearly indicate the extent of private ownership of space round buildings by defining the boundary between private and public space
- Providing a visual buffer between the public realm and internal rooms of individual properties
- Contribute to the legibility of an area in terms of its character;
- Create an important interface between inside and out;
- Encourage activity on the street
- Aid in generating a visible sense of ownership of the street and neighbourhood gardens
- Ensure the appropriate treatment is used to provide activation onto the public realm



Detailed consideration of the treatment of plot boundaries is essential to ensure a safe and attractive environment is created for the future residents of SSGA.

The space between the building and the pavement and between private gardens and neighbourhood gardens, should provide a sense of ownership and privacy for residents as well as encourage opportunities for social interaction among neighbours. Each type of boundary within the SSGA will require a different treatment; this is discussed further in the Masterplan and Design Code in Chapters 9-10.

Public Realm

Designs should maximise the physical and visual relationship between the urban environment and the distinctive assets of the surrounding natural landscape: its landmarks, vistas and ecology;

All streets, squares and parks should be clearly defined by and overlooked by appropriately-scaled buildings and should benefit from the natural surveillance this relationship generates;

SSGA’s new public realm must provide the high-quality and attractive recreational spaces required by its residents, as well as contributing to connecting the area with its surrounding landscape. The public realm is therefore of strategic and local importance, its design should be guided by a number of overarching principles, as well as by respect for context and the needs of specific features.

Hard and soft landscaping should be located in areas which provide passive supervision and be constructed to a high standard using durable high quality materials.



The contribution of street furniture to the public realm is vital. This includes street lights, bollards, signage, railings and litter bins. The choice and positioning of all street furniture and materials should be carefully considered.

Built Conservation

Development within the vicinity of Ryhope Pumping Station and Burdon Hall will be required to be designed to make a positive contribution to the heritage assets significance and setting.

Geophysical surveys and archaeological trial trenching will be required from all SSGA sites.

Archaeological work will be required to investigate the Historic Environmental Record (HER) sites of the windmill and lime kiln.

Several of the sites within SSGA are adjacent or in close proximity to Ryhope Engines Museum, a Schedule Ancient Monument (SAM) and a grade II* listed building, its grounds also include the grade II listed Cottages and Gatehouse. The Ryhope Engine Museum site is highly significant in heritage and historic landscape terms with immediate and wider landscape significance.

Shorter and longer distance views of the museum are of particular importance as the tower of the pumping station can be seen from miles around.

Development within the vicinity of Ryhope Engines Museum should be supported and informed by a Heritage Impact Assessment/Heritage Statement. Overall proposals will need to demonstrate that development will not adversely affect the significance of the Scheduled Ancient Monument and Listed Buildings, but instead better reveals or enhances their settings, in particular views to and from the heritage assets.

There are a number of Heritage Environment Records (HER) within SSGA, two of particular interest are the Windmill and Lime Kiln HERs as such archaeological works will be required to further investigate these sites.

7.3 Residential development – housing mix and choice



Objective 3

To deliver high quality large family housing and wider housing choices.

The SSGA has been identified in the CSDP as a growth area to provide much needed new housing; new housing that extends the current housing offer in the City and provides housing options that are not currently available. The Strategic Housing Market Assessment (SHMA) 2017 highlights that the city continues to lose population because, amongst other things, there is a lack of housing choice in the city with a particular lack of family dwellings. The development of SSGA seeks to address this imbalance.

Residential development guiding principles and requirements

Based on an analysis of the SHMA and the Strategic Housing Land Availability Assessment (SHLAA) whilst acknowledging the implications of other policy requirements including the Habitats Directive, guiding principles and requirements have been developed to ensure proposed residential development at SSGA provides the appropriate housing number, mix and choice.

Housing Numbers

Approximately 3,000 dwellings will be provided across SSGA, at the expected housing densities discussed in 'Heights, Density and Form'.

The SHLAA identifies potential housing sites across the City and considers potential capacities, in terms of dwelling numbers for each individual site. Sites within the SSGA have been included in the SHLAA as forming

part of the city's housing supply and are set out in the CSDP, within Policy SS6.

The following table provides details of the potential capacities of each site, based on the CSDP.

Table 6 Anticipated approximate capacities

Site	Capacity
Chapelgarth	750
Land North of Burdon Lane	1,000
Cherry Knowle	800
South Ryhope	450

These numbers do broadly take into account the local landscape, habitat buffer zone requirements, the aspirations of the council for larger family dwellings and the need to satisfy the Habitat Regulations. The above indicative numbers should be used as a guideline when considering the number of dwellings proposed on any of the SSGA sites.



Housing mix

Residential development is to comprise of a mix of dwelling types, sizes and tenures, reflecting housing need identified in the SHMA.

The SHMA, reviewed every 4-5 years, identifies the housing needs and requirements of the city. As the SSGA is expected to be planned and constructed over 15-20 years, housing needs and aspirations may change. As such, applications should be informed by the most up-to-date SHMA.

Larger family dwellings

Development proposals should include an element of larger family dwellings within each site. With particular emphasis on the provision of these at Chapelgarth.

The council do not have a specific definition for larger family dwellings in terms of plot size, number of bedrooms, or amount of private amenity space. However, the SHMA highlighted that high income households seeking housing in Sunderland are looking for larger detached properties with four or more bedrooms in desirable locations and of high quality construction.



Affordable housing

10% of residential development should be affordable with a tenure split of 75% social rented and 25% intermediate. All affordable housing should be integrated seamlessly within the rest of the residential development.

Affordable housing should be on site, however where it can be robustly justified, off site provision may be accepted. The affordable housing which should be indistinguishable in terms of appearance from the market housing should be dispersed in clusters of a size proportionate to the scale of the development.

The type/size of affordable housing should reflect the needs identified in the most up-to-date SHMA. All affordable housing should remain affordable in perpetuity.

Accommodation for older people/or Retirement Village

Specific accommodation for older people/retirement village should be provided at Cherry Knowle. Proposals for additional specific accommodation for older people/retirement village accommodation within SSGA will be considered on its merits and in accordance with the need requirements at the time of consideration.

Developments should consider alternative designs and layouts to provide for those older people who may want to stay in their own home and take on board appropriate evidence to ensure suitable accommodation for older people is provided where needed.



Housing type

There will be variety in the housing types provided at SSGA to offer choice. Imaginative and high quality development both in terms of housing design and materials will be required. Developers will need to demonstrate how they have captured the spirit of innovation whilst moving towards more sustainable ways of living within their proposals.

Our evidence (SHMA) highlights that a better range and choice of quality housing is needed to improve the housing offer. It is essential to plan to meet the needs of all parts of the community. These needs include affordability for first time buyers or those who wish to move, for families and for the growing ageing population. Consideration should be given to the design and layout of all new homes and the future needs of those groups who need specialist housing.

This will help provide interest in the character and design of SSGA and should include larger family dwellings with high specification and construction. Elsewhere imaginative use of town houses, terraces and other forms of high quality, but higher density housing types will prevail. In the interests of providing a range of housing at SSGA, it is also desirable for land to be made available for an element of self-build, custom build and co-housing projects.

Self build

The City Council will encourage the inclusion of self build properties within SSGA. Proposals which include areas designated for self build units will be required to be accompanied by a design code when submitted for planning permission.

Self build and custom build housing provide housing which is in tune with peoples specific housing requirements. Central government is keen to support and encourage individuals and communities who want to build their own home. A detailed design code will need to be submitted to ensure a coordinated design response between properties.



Future proofing

It is anticipated that development of SSGA will continue for 15-20 years. During this time housing needs and policies will change therefore proposals must demonstrate a housing mix in keeping with the most up-to-date of the SHMA and Local Plan.

7.4 Thriving communities - social infrastructure



Objective 4

Provide new facilities including neighbourhood centre, primary school and open space where the greatest number of new and existing residents can access them easily and safely

To ensure SSGA is a truly sustainable thriving community, it is necessary to ensure that adequate services and facilities are in place to meet the needs of the new and existing residents. Just as with the settled community, those choosing to come to SSGA will have an expectation of investing not just in a house, but in all aspects of a new place. It is the whole package they are choosing- home, neighbourhood environment, shopping, schools, leisure, parks.

Social Infrastructure should aid social interaction and create a healthy, inclusive, thriving community at SSGA. Opportunities exist to provide new facilities which complement those already present in the nearby neighbourhoods.

Key Principles

Based on an analysis of the council's sustainable neighbourhoods study, educational forecasting and conversations with key services providers there is a clear understanding of the social infrastructure required as a consequence of development at SSGA.

- The provision of community facilities within SSGA is key to the creation of place, rather than simply a collection of housing estates.
- The integration of these facilities needs careful consideration in terms of their design and the important inter-relationships with each other and new residential neighbourhoods.

Thriving communities guiding principles and requirements

Neighbourhood centre and local parades

A small neighbourhood centre will be created at Land North of Burdon Lane to provide an accessible focal point for the community with complimentary smaller local provision (local parades) at Chapelgarth, Cherry Knowle and South Ryhope.

In order to minimise the need to travel and to ensure future residents are able to meet their day-to-day needs locally, a small neighbourhood centre will be required within Land North of Burdon Lane, at the heart of SSGA (approximately 1ha in size for A1/A3/A4 uses which includes car parking provision). Smaller local provision will, where viable, be required within the other three sites to serve the needs of the local residents, this may take the form of local parades.

Individual A1 food stores are not to exceed the threshold of 500m² gross area individually to provide an appropriate level of convenience goods to meet the needs of new residents and complement rather than compete with existing centres. (An impact assessment will need to be submitted if the total amount of A1 provision exceeds 500m² with the neighbourhood centre) in accordance with the Local Thresholds set out in Policy VC2 of the CSDP.



Uses appropriate for a Neighbourhood Centre include:

- A1 - Shops
- A3 - Restaurants and cafes
- A4 - Drinking establishments
- D1 - Non-Residential Institutions i.e. Places of Worship, Health Centres, Clinics
- D2 - Assembly & Leisure

Ryhope and Doxford will continue to act as the district and local centres with a wider range of shops and services. The new homes will assist in sustaining and improving existing services and facilities in these centres.

Education

The extension of 2 existing primary schools, in close proximity to SSGA, will be required in the short term to meet the needs of the new community.

A new 1.5 form entry primary (with room for expansion) will be required including a nursery/day care in the medium-long term within Land North of Burdon Lane.

The new school should also serve as a community hub, with facilities and services for community use.

In recognition of the inevitable population growth, anticipated as proposals are implemented, there will be a need to provide more capacity within existing primary schools in the short term and in the longer term there is a need for a new high quality primary school, including a nursery/day care within SSGA.

The new school and nursery facilities are to be located centrally within Land North of Burdon Lane and well connected to the surrounding residential developments by pedestrian and cycle routes. The school/nursery should directly relate to the new areas of public open space for both recreational and learning benefits as well as enabling the school to be safely accessible by foot and bicycle from the whole of the SSGA.



School playing fields and play spaces should be located within the GI corridor, but not the SSAANGs and be accessible for use by the public out of school operating hours.

Sunderland is currently in a position where surplus provision exists within the secondary sector. However, recent increase in the local birth-rate, leading to increased demand for local primary places, has eroded

this in recent years. Places have become limited in Venerable Bede Academy in recent year 7 admissions rounds with a reduced number of places in Farrington Academy. While it is not anticipated that a new secondary school will be required to meet the demand for secondary school places as a result of the South Sunderland growth area it is expected that some capital modification will be required to increase the capacity at one or both of the secondary schools serving the development and contributions will be sought to reflect this requirement.

In order to promote safe and sustainable access to existing secondary schools outside the SSGA, developers will be required to contribute to the provision or enhancement of safe pedestrian and cycle routes, where it is deemed necessary.

In terms of school place requirements, evidence is suggesting a need for Special Educational Needs (SEND) provision, as proportionally the number of children is anticipated to increase. Further work is being undertaken on this to identify an appropriate location and cost accordingly.



Art

The inclusion of art will be actively encouraged as part of the new development or the open landscape.

Art has an important role in creating a positive local identity as well as bringing delight and appreciation to the everyday experience of a place. Opportunities exist for incorporating art works into new residential development, shopping facilities, community facilities and areas of Green Infrastructure.

Art proposals should:

- Relate to the area, its values, culture and people
- Be easily visible and accessible to the public

- Help to create a place of congregation and activity; and establish landmarks and neighbourhood gateways.

Community facilities

As the surrounding area already benefits from a number of community facilities, there will be no requirement for additional stand alone facilities within SSGA. However, it is expected the new school at Land North of Burdon Lane and the specific accommodation for older people/Retirement Village at Cherry Knowle become hubs, the hearts of the community with facilities and spaces available for community uses.

Healthcare

The NHS have not indicated that there will be a requirement for additional surgery provision within SSGA as there are already surgeries within Silksworth and Ryhope that have sufficient capacity to accommodate the new community. However, the City Council are currently liaising with the NHS (Clinical Commissioning Group) with regards updated information on surgery capacities and are awaiting updated figures, as such any requirement for additional surgery provision may be requested through individual planning applications, however this will be subject to viability.

Health centres are appropriate uses for inclusion within the Neighbourhood Centre should a need arise in the future.

7.5 Movement



Objective 5

To create development which is well connected to the surrounding area and facilities by road, footpath, cycle route and public transport links.

All parts of the SSGA should be accessible by foot, cycle and public transport, with networks designed to promote these modes over private transport, reflecting the saved policies of the UDP and adopted CSDP.

Key principles

To reduce the reliance on car-based travel, development must provide access to the fullest range of sustainable transport modes. Access to pedestrian and cycle networks and public transport will be crucial. It will also be important to consider how the impact of private cars can be minimised and as Ultra Low Emission Vehicles (ULEVs) increasingly becoming an attractive alternative, following some significant advances in charging technology.

Movement guiding requirements and principles

Sustainable Communities

Provide a well connected and permeable network of streets and open spaces that enhance the attractiveness of walking, cycling and public transport journeys.

To ensure the creation of a well-connected and permeable network developers should demonstrate how the application of principles from 'Manual for Streets' and Building for Life 12 have been incorporated into development design proposals for residential streets

It is important to recognise how the design of streets within the urban extension, in terms of their connectivity and permeability, will have a direct influence over how people choose to travel. The creation of a well-connected internal network of streets and open spaces and good external connectivity to surrounding highway networks will need to be demonstrated. This will need to be particularly evident in terms of access between the SSGA and Doxford and Ryhope centres to ensure walking and cycling as opposed to private car use, represent the most attractive travel option for journeys between these two areas.



The application of these principles will help to create a safe, highly accessible development that will both facilitate and encourage more journeys to local destinations by walking and cycling.

Pedestrians and Cyclists

Actively encourage walking and cycling journeys through the provision of safe, attractive and direct routes to key destinations

Pedestrian links should be developed to complement the needs of the new residents in terms of connecting to key destinations such as schools, employment and leisure facilities. Pathways, pavements and shared surfaces all combine to create the pedestrian network, these should be closely linked with areas of green infrastructure to create pleasant walking experiences. Priority should always be given to accommodating these movements over vehicular traffic.

Countryside recreational paths should also be provided through the GI corridors, SSAANGs and where feasible as part of buffer zones. In seeking to reduce the number of people visiting the coast for recreational purposes, paths should not increase accessibility to the coast.



Cyclists will generally be accommodated on the street network (on road). The streets and cycleway will connect the new community to the local and wider urban context and providing routes to significant destinations.

Connections should also be made into the existing cycle networks that skim the SSGA, including National Cycle Network Route 1 and the Walney to Wear (W2W). Improvements and where necessary potential realignment of these routes may be required.

Further detail regarding necessary pedestrian and cycle network improvements are detailed in the Design Code (Chapter 9).

Public Transport

The provision of new and improved public transport services and infrastructure will be required to support the development of a sustainable community.

The majority of proposed development at SSGA is not currently accessible by public transport. There is a need to penetrate bus routes into the new development areas. Nexus, advocate the need for public transport services to be firmly established from the outset so as to help reduce car dependency.

To promote public transport development proposals must be co-ordinated and provide for a bus route. Provision for a convenient and attractive public transport service should be incorporated into all developments. Estate roads should be of a suitable 7.2 metre width and construction standard to accommodate a bus service, the Indicative Masterplan Chapter provides details of these and provides an indication of bus routes. Developers should consult with the City Council and Nexus at the earliest opportunity to discuss appropriate routes

Residential properties should be within 400 metres of a bus stop with a frequent service to ensure the service represents an attractive proposition for commuters. Bus

stops and necessary travel information should be provided at suitable places throughout the growth area.

To assist in the early development of an attractive bus services to the first phases of development it will be necessary for developers to contribute to the required level of service on these bus routes for an agreed set period of time. Developers should actively market bus services to new residents to help maximise patronage.

Transport Assessment and Travel Plan

Developers must prepare a Transport Assessment and Travel Plan.

A Transport Assessment will need to consider the transport implications of proposed developments at SSGA on the local and strategic highway network and neighbouring local authority roads, particularly those in Seaham, Durham.

Where issues are apparent as a result of development proposals, designs solutions for the upgrade of these locations will need to be derived and funded by the developer.

A Travel Plan must demonstrate how sustainable travel patterns will be actively encouraged and delivered through the development proposal.



Parking

Include a level of parking, Ultra Low Emission Vehicles parking and cycle parking appropriate to the development; every effort should be made to avoid vehicles dominating the street scene.

Parking must be well designed and integrated into the development and contribute to the overall quality of the public realm. Proposals should create an environment which is not dominated by car parking and follow the principles set out within the Interim Development Management Planning Guidance (excluding spacing standards 10G), Manual for Streets and Building for Life12.

A variety of methods of car parking should be provided throughout the growth area, these should be dependant upon the character area in which they sit. Appropriate arrangements may include a combination of on-street parking within well-designed public realm spaces, in curtilage parking or within a courtyard. Developments should avoid the dominant use of integrated garages and areas of hard-standing for parked vehicles which front the street in order to create a high quality public realm, creating a safer pedestrian environment.

Parking for visitors should be evenly distributed throughout the SSGA with each space in reasonable proximity to the group of dwellings it is intended to serve.

Sufficient cycle parking should be implemented to actively encourage cycle journeys to the neighbourhood centre, school and leisure facilities. Cycle parking should be located in areas with good surveillance.

Electric vehicles and Ultra Low Emission Vehicles (ULEVs) are a more sustainable mode of transport; their use within the SSGA should be encouraged through provision of dedicated parking for ULEVs and ensuring new dwellings make provision for the installation of home charging apparatus. Proposals that link residence with a commitment to use ULEVs or membership of car clubs will be especially encouraged.

Non-residential developments, including the new school and neighbourhood centre should also provide dedicated ULEV parking spaces and electric vehicle charging points.

Local/Strategic Connections

New local/strategic connections and works to existing local/strategic connections will be required to help link SSGA with its surrounding areas and reduce the impact of development.

Connection/works required include, but not limited to:

- A19/A690 Junction Improvements
- Southern Radial Route
- Ryhope Doxford Link Road
- B1286
- Burdon Lane
- Nettles Lane
- Burdon Road



A number of new local/strategic connections and improvements to existing local/strategic connections will be necessary to ensure SSGA successfully integrates into existing neighbourhoods, provides convenient connections to the wider area and does not negatively impact on the surrounding areas.

Below is a summary of the Local and Strategic Connections, further details on these can be found in the Indicative Masterplan.

Strategic Transport Proposals

A19/A690 Junction Improvements

SCC have secured Pinch Point funding to implement junction/signalling improvements on the A19/A690 junction, these measures should alleviate any concerns that Highways England may have regarding development at SSGA and as a consequence increased congestion at key junctions on the Strategic Highway.

Local Connections

Southern Radial Route

The Southern Radial Route will provide access to South Ryhope. One access point off the roundabout. An exit only junction from the development site on to the Radial Route.

Ryhope Doxford Link Road

A carriageway which will link the eastern end of Doxford Park to the Southern Radial Route. The development of the Ryhope Doxford Link Road (RDLR) is a complex process, bringing together a variety of built development with the necessary roads, services and landscape infrastructure.

Several sections of the road are already in place, as identified on Fig 21 and others are being implemented by developers, only one section of the road remains to be funded and delivered- the 'missing link'. The 'missing

link' will connect Cherry Knowles and Land North of Burdon Lane and complete the RDLR. For gradient and amenity reasons it is expected that the 'missing link' will be in a cutting where it crosses Burdon Lane and as such a bridge suitable for horse riders, pedestrians and cyclists will be necessary. No junctions to Burdon Lane from the RDLR will be permitted.

The RDLR will intercept areas of public green space and SSAANGs, therefore careful and sensitive design will be required to ensure that the highway is not dominant on the visual landscape. Appropriate crossings will be required to ensure the RDLR does not act as a barrier for movement.

Contributions will be sought for the 'missing link' the one remaining section of the road. Contributions to be sought from all development sites within SSGA. The Infrastructure Delivery Strategy provides further detail on the RDLR and the contributions required.

B1286

The original route for the RDLR proposed an extra leg off the roundabout junction of the Doxford Way/ Burdon Road, however it is felt that an extra leg of this roundabout would not be feasible, therefore a new roundabout is proposed on the B1286 to provide continued access along the RDLR through to the Southern Radial Route. However the exact alignment of the RDLR is yet to be determined and will be subject to preliminary site investigation, which will be undertaken as part of any planning application.

Burdon Lane

No access to be permitted off Burdon Lane once the RDLR is complete and operational.

Burdon Lane is to be severed where the Ryhope Doxford Link Road (RDLR) crosses the lane at the Cherry Knowle site. The lane will no longer provide vehicular access until beyond Nettles Lane. Fig 21 details where the lane is to be severed. The severed section will become a multi-user route for pedestrians, cyclists and horse riders.



Nettles Lane

To be retained as a Multi User Route, although it may be necessary for the lane to be intercepted by highway with appropriate crossing facilities. Hedgerows bound the lane on the northern and southern ends of the route; these will be retained and maintained.

Burdon Road

The only access point on Burdon Road (to the south of Lodgeside Meadows) will be to serve the South West corner of Land North of Burdon Lane. This should be a localised access with no through route and serve a maximum of 100 dwellings, unless agreed otherwise with the Local Planning Authority.

Route Hierarchy

Provide a high quality and legible hierarchy of streets which contribute to the wider objectives of SSGA.

Hierarchy to include:

- Primary Vehicle Route: 7.2 m (Plus 2m Pedestrian Footpath or 3m shared pedestrian/ Cycle route)
- Secondary Vehicle Route: 5.5m (Plus 2m Pedestrian Footpath)
- Shared Surfaces/Homezones: 4.5-5.5m
- Pedestrian/cycle only links: Minimum 3m – (not attached to highway)

The inclusion of an easy to interpret route hierarchy will provide a legible development each having different design characteristics in terms of function, width, building form, landscaping and frontage details. The Masterplan provides details on the different types of routes identified and the design principles which should be applied to all development proposals throughout the SSGA.



7.6 Sustainable development



Objective 6

To deliver a sustainable community that cares for the city's environment, makes efficient use of natural resources and mitigates against climate change.

Sunderland is committed to playing its part in tackling climate change in helping to achieve the national target to cut carbon dioxide emissions by 80% by 2050. The Council declared a climate emergency in March 2019 and is seeking initiatives that will deliver carbon neutrality by 2030. SSGA has a key role to play too, sustainable design and construction will assist in reducing the impacts of climate change and ensuring the area is resilient to the potential effects.

Development of the SSGA offers an opportunity to address some of the noted sensitivities within the area including surface water flooding and drainage.

Key principles

Based on an analysis of the councils Renewable Energy Study for SSGA, discussions with utility providers and an understanding of the area and the city's sensitivities, the objectives of Sustainable Development are driven by some key principles

- SSGA will provide warmer and more energy efficient homes that are cheaper to heat.
- The new neighbourhood will be designed to be as energy and resource efficient as possible - across the development time line and in full life cycle terms.
- Reduced carbon levels will be achieved across the site.
- Development of the area will take account of Magnesian Limestone Aquifer
- SUDs will influence the design and layout of development proposals.

Sustainable development guiding principles and parameters

Renewable Energy/Energy Efficiency

The creation of a sustainable low carbon community is a priority at SSGA:

- All developments should seek to achieve or exceed central governments targets towards sustainable construction.
- Thought should be given to the layout and positioning of developments to optimise renewable energy technologies and solar gain.
- Wherever feasible green roofs should be incorporated.

Although the code for sustainable homes is no longer the national standard for sustainable development the City Council will still encourage both residential and non-residential developments to be constructed to a higher sustainability standard than that provided within building regulations.

In order to ensure the energy efficiency of properties the layout of developments should be designed to reduce dependence on energy for heat and lighting through maximising the southern orientation of buildings and enabling passive solar gains and the use of micro-generation technologies such as PV panels.

The main glazed elevation should be orientated within 30° of south ('solar orientation') with a correspondingly small proportion of glazing on the north elevation. Buildings should also be designed to enable the integration of solar panels into the roofscape. Consideration should also be given to potential overheating through careful use of solar shading, where appropriate.

Inevitably, road layout will influence the arrangement of buildings on a new development, with east-west alignment enabling the optimal orientation of houses for passive solar gain. However, it is not essential for all buildings to be orientated due south and variations of up to 30° can be accommodated whilst still benefiting from passive solar gains. On north-south roads, detached units provide greater flexibility for maximising solar gain.

Every effort should be made to ensure a large proportion of the roofscape has an orientation within 30° of due south so solar renewables can be retro fitted if they are not incorporated as part of the original design solution. The use of other micro-generation technologies such as ground source heat pumps should also be explored.

Green roofs aid in adapting to the effects of climate change through reducing the need for artificial cooling, attenuating/capturing rainwater runoff and the potential to provide habitats. The inclusion of green roofs throughout the growth area will be encouraged and looked favourably upon particularly in areas with issues with surface water flooding.



Building materials should also be sustainably sourced where possible and have a reduced environmental impact whilst requiring low maintenance and are durable to the UK climate.

Water Management

- **Flood Risk**
All development proposals will be required to consider the effect of the proposed development on flood risk, both on-site and off-site, commensurate with the scale and impact of the development, through the completion of a Flood Risk Assessment (FRA)
- **Sustainable Urban Drainage (SUDs)**
Development at SSGA will have to ensure integration of Sustainable Drainage System (SUDs) to manage surface water drainage. Where SUDs are provided arrangements must be put in place for their whole life management and maintenance. SUDs should contribute to the provision of Green Infrastructure.

The growth area is situated on the Magnesian Limestone aquifer and most of the area is within a groundwater source protection zone. Therefore, any development in this area must not have a negative impact on water quality. In addition to this, the growth area is located in three Water Framework Directive river catchments: the Doxford Park to Hendon Coastal Area water body, the Ryhope Coastal Area water body and the Burdon to Ryhope Coastal Area water body.

Surface water runoff caused by development in the growth area has to be sufficiently managed in order not

to adversely affect the water quality in these catchments, this will also be necessary in satisfying HRAs.



In addition, the area is sensitive in terms of drainage and parts of the growth area are in a Critical Drainage Area. It is imperative that the development has an integrated sustainable drainage system (SUDs) that effectively manages water quantity and improves water quality. The drainage requirements of the site should be informed by a detailed assessment of the existing drainage regime in the area and a site Flood Risk Assessment.

SUDs features should be considered in the earliest stages of the site design. The use of SUDs will be an important part of the drainage infrastructure and landscape structure of SSGA. The SUDs should aim to produce wider benefits for amenity and biodiversity. Any development will need to consider the relationship with SUDs as a key leisure and visual amenity asset.

The types of SUDs that will be appropriate will depend on the load and soil conditions of the individual sites.

Utilities

Adequate utility infrastructure must be provided to serve each phase of development



Future proposals for development within SSGA will need to consider connection to utilities infrastructure at the earliest opportunity. Surveys may be required as part of any detailed design proposals and all development proposals will need to be informed by discussions with utility providers to ensure that connections to proposed development can be made to provide water, gas, electricity, sewage, telecommunications etc.

Any required sewer network upgrades should be undertaken before or in line with the development to ensure that the water quality of the aquatic environment is protected.

Contaminated Land

Site contamination and ground condition surveys will be required to inform any detailed design proposals

Identified historical and current activities within the SSGA area may have resulted in contamination. Land remediation or mitigation measures may be required. If potentially contaminative land uses are identified it is the responsibility of the developer to assess the level of risk posed by the contamination to controlled waters receptors e.g. surface watercourses and groundwater. Identified risks posed to controlled waters should be addressed and mitigated/remediated where necessary

8 Concept masterplan

Working within the principles and parameters set out in the previous chapters a concept masterplan has been prepared. This represents the council's interpretation of how the key components of the Principles and Parameters Chapter may be realised in a spatial form across the area.

The concept masterplan recognises the key objectives of the SSGA SPD, takes into consideration the individual site constraints and opportunities identified in the SWOT analysis and adopts the development principles. In doing so, the spatial masterplan offers a clear indication of how a strong urban design agenda and sound design principles can be realised across the area.



Fig 22 SSGA Masterplan

9 Design code

9.1 Purpose and status of code

This code aims to achieve a balance between a clear level of prescription to ensure high standards of design and an appropriate degree of flexibility to allow designers freedom to bring forward innovative and imaginative proposals that improve upon the code and importantly, to accommodate changing needs, market conditions or government/local guidance over the duration of projects.

Proposals should seek to accord with the code, where this is not achievable clear justification must be provided, detailing how the alternative design solution will not undermine key design objectives.

9.2 Movement and Streets

The following section outlines the movement and street proposals, which should be applied to all development throughout the SSGA. Whilst the development of the built form is of major importance, of equal importance is the layout, hierarchy and quality of streets and routes running through the SSGA. The way in which the

movement network is provided within developments is central to creating an easily accessible and safe community and will influence the overall design of an area.

To ensure developments meet the objectives of the SPD the provision of interconnected streets and routes which promote sustainable modes of transport and integrates with existing communities is essential. The aspiration of the SSGA is to increase the opportunity for existing and proposed residents to walk and cycle along attractive routes, connecting directly with all-important destinations within the development and beyond.

The movement network is illustrated below.

Pedestrians and cyclists

A network of interconnected pedestrian and cycle routes will be provided throughout the SSGA. The network will provide both direct routes within the built environment and recreational routes located within the green infrastructure.



Fig 23 Movement

In order to ensure that walking and cycling are the natural option the masterplan has been developed to provide a high level of permeability through the use of perimeter block development. Where development blocks do not form this style, pedestrian only routes or green links have been provided.

To ensure all routes are well used and fit for purpose they will be designed with adequate levels of natural surveillance and activation creating a safe environment.

Urban routes

Routes within the urban environment to be constructed of durable materials which enhance the public realm and are low maintenance.

Recreational routes

It is the aim of the masterplan to create a semi-rural development where residents can walk/cycle through the entire area without needing to travel along a primary street or into the main urban environment. Therefore a network of recreational routes will be created, these routes will provide a pedestrian/cycle route from Chapelgarth to South Ryhope through a series of green links and park walks interspersed with green space. The routes will also provide connections to major trip destinations such as the local centre, retail facilities and schools.

Green links



- Existing hedge rows and tree belts and their associated buffer zones will be utilised to create green links, these routes will also form part of the recreational movement network to encourage walking and cycling by residents. The green links will provide north – south and east- west connections across the entire SSGA and be punctuated by areas of amenity green space, play space, SSAANGs and public spaces.

- Due to the routes being located within the buffer zones for natural heritage assets the routes will provide habitat creation and aid in protecting/encouraging biodiversity.

Park walks

- Located within the amenity green spaces and larger parks (SSAANGs) the routes will meander through spaces creating opportunities of recreational purposes including dog walking.
- The materials chosen for the construction of the routes will be important in the overall 'feel' created; the paths should blend into the landscape. Therefore tarmac will not be considered an appropriate material choice.

Multi user routes



Multi-user routes for pedestrian, cycle and equestrian activities to be provided in several locations throughout the SSGA, including:

- Nettles Lane
- Section of Burdon Lane.
- W2W route to the south of Chapelgarth
- Length of RDLR from South Ryhope to North of Burdon Lane.

The routes will be a minimum of 3m in width and contain a 1m verge on either side

Way finding signs should be provided on pedestrian and cycle routes to provide essential information to users on the routes available, destinations and distances.

Public transport corridors

Public transport corridors will be provided through each subarea of the SSGA and connect with existing services within the area. The routes will be designated to take residents in the most direct routes possible to key destinations within the city.

The location of the corridors will provide a bus service within 400m of all residents within the SSGA. The routes will be direct through the development and where possible not require the bus to double back.

Bus only links will also be provided to allow for a speedier service, however this must not be to the detriment of the natural or built assets of the SSGA.

To provide an interconnected sustainable transport network and encourage the use of the bus service, green links will connect up to and dissect the public transport corridors.

The corridors must be a minimum width of 6.7m and located on the Primary Street type. To ensure residential parking does not obstruct bus movement a 2m wide on street parking/landscaping verge will be provided on bus corridors. For further details see the Primary Street type. Below are details on corridor locations

Chapelgarth

- Access will not be permitted from the roundabout on Moorside road into Chapelgarth due to the current vehicle levels, number of 'arms' already provided on the junction, the existing tree belt and hedgerow.
- The designated natural heritage asset Blackney Woods LWS also prevents a corridor linking into the existing residential area on Hall Farm Road.
- The public transport corridor will therefore utilise the two roundabouts on Weymouth Road as an entrance and exit and 'loop' through the site.

Land North of Burdon Lane

- The majority of the site is accessed via a new junction on the RDLR, a secondary access is provided from Burdon Road, however this will not connect through to the remainder of the site for private vehicles.
- In order to provide a public service corridor which entails a route that does not double back on its self a 'bus only' link will be provided from the land in the south west (east of Burdon Road) through to the remainder of the sub area, private vehicles will not be permitted through this route.

- Providing a bus only link will allow for a corridor which serves the whole of the sub area and connects to the RDLR.
- The route could potentially run through to Rushford phase 2 and onto Ryhope Street (local centre) to provide a more direct service.

Cherry Knowle

- Corridor to be accessed via junction with the RDLR opposite Hopewood Park, the corridor will then run through the centre of the site and exit back opposite Waterworks road.
- The service can either link up with Waterworks Road and Stockton Road and connect into existing provision in Ryhope or return to the RDLR/Stockton Road junction.

South Ryhope

- Accessed via the central roundabout on the RDLR
- The corridor will run through the centre of site and cross the central green spine.
- The limitations of access to the site create a 'cul-de sac' style development. To remove these restrictions and provide an improved transport service a 'exit only' connection can be provided onto the RDLR at the north west of the site.

Ryhope Doxford Link Road

Careful consideration needs to be given to the design of the RDLR to ensure it is not overly engineered and is characterised by more of an urban residential route than the existing section at Cherry Knowle. The highway will be required to be high capacity to relieve through traffic from existing communities therefore consideration must be given to the following:

- A landscape belt of approx. 15-20m is required between the RDLR and proposed development
- Landscape belt to include cycle and pedestrian route
- Private accesses and parking are not permitted directly from the RDLR
- Shared surface areas will be required, these can be located within the landscape belt.

Primary Streets

Primary access routes will accommodate a variety of functions including bus routes, frequent vehicle movement, multi-user routes and pedestrian footpaths. The character of the highway will be urban in nature consisting of a wide avenue.

In order to prevent parked vehicles blocking the highway and pedestrian routes a 2m wide channel of parallel parking bays interspersed with landscaping will be provided on both sides of the carriageway.

A multi-user route will be provided on one side and will be wide enough to provide both pedestrian and cycle movement (3m). The route will be located between the parking/landscaping channel and residential properties front garden space. A standard public footpath will be provided on the alternative side of the carriageway.

Key dimensions:

- Wider feel to the highway 24-25m.
- 6.7m highway, allow for 2 buses to pass.
- 2m verge between highway and pavement, verge will include a combination of parallel on street parking and landscaping.
- 3m pedestrian and cycle shared paving to be provided on one side
- 2m pedestrian route provided on one side.
- Front garden or Narrow front garden set back and boundary treatment.

Secondary Streets

Providing the principal access off the primary street to residential properties, secondary streets will have a strong urban character and a supporting role to the primary street is helping to define the main vehicular routes.

These streets are unlikely to carry large volumes of traffic or bus routes and consequently allows for an approach towards design which is less focussed on accommodating through movement. Properties will provide frontage and a strong building line onto the streets with direct pedestrian access to the street from the property. A design response which provides the back of properties onto public streets will not be considered appropriate.

Key Dimensions:

- Width 17 - 19.5m

- 5.5m highway
- 2m pedestrian footpath
- Front garden or narrow front garden set back and boundary treatment (see set back and boundary treatment)

Green Streets

Streets facing onto or adjoining open spaces may take various different forms depending on the type of open space which the street fronts onto. These streets should be designed for low traffic flows enabling a mix of pedestrians and vehicles. In the main, these will be located on the urban – rural fringe where development bounds green space, SSAANGs, green links and natural heritage features. The routes should provide a high quality public realm where pedestrians have priority and vehicles do not dominate.

Properties will only be located on one side of the highway with green space on the other therefore the below requirements & dimensions are expected:

- No level change between the footway and the vehicle path, however changes in surface materials may be sought.
- Adequate parking facilities for properties to prevent poor parking behaviour dominating the streetscene.
- 4.5-5.5m carriageway.
- 4m minimum residential front garden.

It is anticipated that the green streets will comprise of either shared surfaces or Home Zones, these are detailed below

Shared Surface

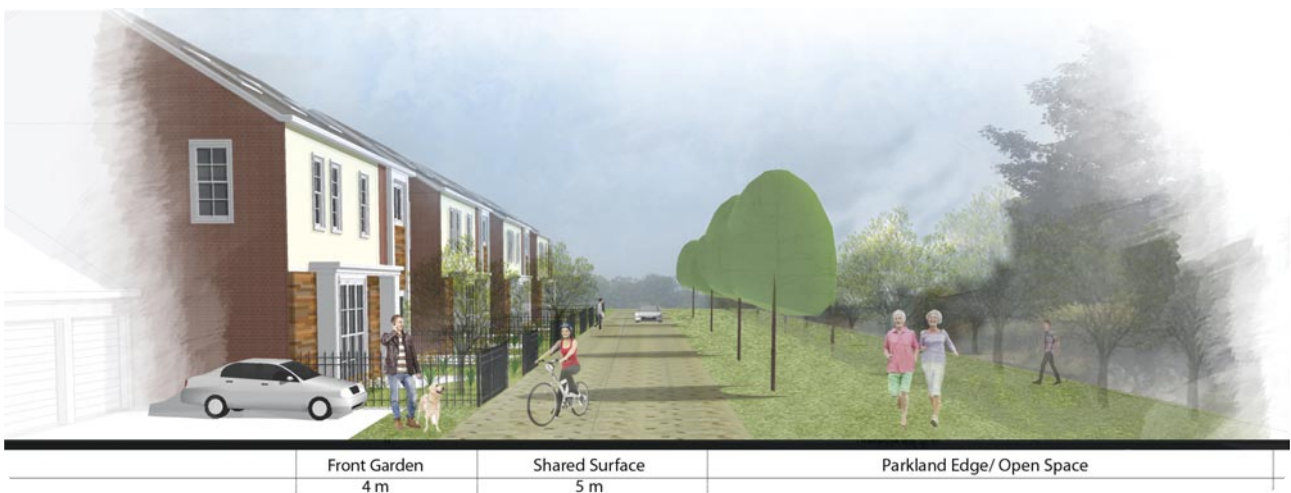
This street type includes shared spaces designed for very low vehicle speeds which should be self-enforcing through good design. They provide access for small groups of homes either in courtyard form or short streets. Shared space streets should help define the building blocks and will be part of the permeable network of routes. Within a cul-de-sac this type of street can be used to serve up to 10 dwellings. There are likely to be higher levels of pedestrian and cyclist activity (particularly from children) along these streets and the design should ensure that these activities are as safe as possible. Even the most vulnerable pedestrians and cyclists should be able to share the whole street safely with vehicles.



Primary Streets



Secondary Streets



Green Streets

Some designated pedestrian routes may be provided, but should not be designed as footways which have full height kerbs and therefore fully segregate vehicles from other street users. Where provided these routes should be free from car parking and adequately provide way finding methods which are suitable for blind and partially sighted people. The routes should be provided in a contrasting coloured material, with a 30mm kerb up stand with flush crossing points and tactile paving where required.

Key Dimensions:

- Width – Varies
- 4.5-5.5m shared surface
- Set back of the property and boundary treatment will depend upon the character of the area and the surrounding features.

Homezone



Home Zones are an urban design led concept for residential developments where streets are intended for a range of activities and are primarily places for people, not places for vehicles. The aim of Home Zones is to improve the quality of life for residents and this takes priority over ease of traffic movement. Streets in Home Zones will include seating and play areas, shared surfaces, parking spaces and areas of planting as well as indirect traffic routes. The streetscape should be aesthetically pleasing and co-ordinated and should promote a sense of unique identity. By reducing the dominance of cars and increasing the use of streets for other activities, Home Zones aim to:

- Foster a sense of community;
- Increase social interaction;
- Increase play opportunities;
- Improve the quality of the built environment;

- Increase natural surveillance, deterring casual crime.

Home Zones are a specific and exacting form of development, with only limited scope for compromise on elements of the design approach. For detailed information see 'Home Zone Design Guidelines' – Institute of Highways Incorporated Engineer 2002.

Parking

Cycle parking



Cycle storage must be designed as an integral component of the layout and be provided in key public spaces and within residential dwellings. The design of cycle parking facilities should be consistent and integrated within the surrounding public realm. For residents, the bicycle should be the most readily accessible mode of transport. Storage requirements:

- Within the footprint of the house or in back gardens within a covered lockable enclosure
- In garages with additional cycle storage space. It is essential that cycles can be taken out easily and without the need to remove cars or other cycles first
- Within key public spaces, secure cycle parking and storage should be provided. This includes at the points of entry into parks, as well as at the neighbourhood centre and the school.

Car Parking

A range of parking methods for residential properties will be provided throughout the growth area including:

- In plot – garages, drive ways
- Courtyard
- Allocated on street
- Unallocated on-street visitor parking



In all instances parking must compliment the character area in which it is situated and not dominate the street scene.

All residential properties within the SSGA will be provided with at least one allocated car park space. Larger dwellings will be provided with two off street parking spaces.

Where garages are provided careful consideration should be given to the size, positioning and treatment of garages. Garages must be located behind the building line and not dominate the streetscene



The Interim Development Management Planning Guidance provides detailed guidance on the various types of residential parking options. Car parking within the SSGA should comply with the guidance set out within the Manual for Streets and the Interim Development Management Planning Guidance.

9.3 Built form

Creating permeable and legible housing layouts is a fundamental objective of the SPD, to ensure this chapter 7.2 requires developments to consist of perimeter blocks. This form of development should consistently define the public and private realm, but vary in terms of enclosure, height and density depending upon the character of the area. The arrangement of dwellings within a block structure and their relationship to the street and to other building elements will be influenced by the plan form of the dwelling themselves.

Development block variety

Including a variety of block sizes throughout the development is a good means of creating interest and character across the SSGA, it is advised that developments should provide a block width range of 35-110m and length range of 60-110m.

Small development blocks provide good pedestrian permeability and are most appropriate in the higher density areas located around the neighbourhood centre. Larger development blocks, at the upper end of the above ranges, will be more appropriate in the lower density development areas of Chapelgarth.

Thin development blocks, approximately 35-40m in width, are largely achievable in traditional layouts where private rear gardens back directly onto each other. Care should however, be taken to avoid numerous thin blocks in a row as this can result in little active frontage along side streets. Larger, square blocks can be achieved through the incorporation of short, direct cul-de-sac if the overall development is well connected into the surrounding area and has sufficient pedestrian connections and permeability within it.

Further information and guidance on development blocks can be found in the Interim Development Management Planning Guidance.

Building types and frontages.

The building types and frontage provided within developments will determine how well streets and spaces are animated and defined. To ensure the appropriate responses to the interface between the built form and public realm is created the below guidance will need to be followed.

Landmark buildings



Landmark buildings will be used to define key gateways and provide points of reference to enhance legibility. The buildings should be defined through an alternative approach to architectural design, which makes the building stand out from the other house types. This should include the materials used, the pattern of fenestration and increased vertical emphasis within the facade and/or an increase in the overall building height up to 3 storeys.

Corner turning



To ensure there is adequate activation and frontage and to remove instances of blank elevations fronting onto the public realm the inclusion of corner turning properties will be required. The properties will be distinguishable from other house types through the inclusion of dual aspect frontage with detailing and fenestration included on all elevations which present onto the public realm. The buildings themselves will not be as prominent as landmark buildings, but will help to provide focal points, an increase in the overall building height to 2.5 storeys will also aid in legibility.

Primary frontages



Sections of the masterplan area are particularly prominent and critical to the impact of development and the public realm. Attention will need to be paid to the treatment of buildings fronting onto or adjacent to, green links, green space, amenity open space, SSAANGs, Green Belt and public squares to ensure that these prominent spaces have a building frontage which helps create distinctive quality and character. These frontages will have rich architectural detailing include high levels of fenestration to establish a strong relationship between the built environment and the public realm.

Secondary frontages



Secondary frontages will be composed of simpler architectural design than the primary frontage but will incorporate architectural detailing and a variety of materials to define their role as important element of the built environment.

Views

Designers should consider the impact of scale and topography on long distance views into the housing site from the surrounding area or neighbourhoods and views out of the development. Density should also be considered alongside massing to allow for appropriate views through a site and into a site.

Careful consideration must be given to the interface between the edge of a development and open spaces/parks/countryside and existing properties. Softening the interface between the development and open countryside with appropriate landscaping may be required and a gradation of built form with varying densities or heights around the periphery of a site.

Architectural design

An architectural style is not prescribed for any site, however the architectural design must be based on sound design principles and be derived from robust analysis of the surrounding context.

House types should reflect positive elements of local vernacular but this can be expressed as a modern interpretation and can display local building characteristics such as layout, building proportion, fenestration, detailing, materials, colours and roof profiles.



The careful detailing of architecture is important to lift a simple palette of good quality materials. Quality of materials, detailing and workmanship will ensure a locally distinctive feel to the housing. Linked with this is good landscaping and boundary treatment which will ensure the vision of the master plan is realised.

Examples of how this should be achieved are:

- Simple building form and elevations
- Attention to detailing to window heads, window reveals, cills, canopies and roof eaves details
- Limited variation in building materials and colours
- Attention to building proportions with regards to windows and storey heights
- Subtle variations in roof materials and roof orientation across large sites to add interest and variety
- Variations in privacy setbacks from public realm



The council will expect developers to demonstrate how the positive elements of local distinctiveness have been translated into development proposals.

Where buildings within the surrounding area have little character or positive architectural quality then new development should give a stronger identity to the area through the architectural expression of dwellings, layout of buildings and creation of new spaces and routes.

Transition of styles

The following provides guidance on transitions of housing styles or typologies between, character areas, development plots and either side of the street or along it. It is not intended to prohibit innovative or distinctive architectural approaches, but there is a number of simple devices that help avoid architectural discord or a disparate street scene – these are set out below and must be considered by designers when developing layouts.

What should be avoided:

- Changes in building styles, colours, proportions, scales and streets design should be avoided where character areas meet, on either side of a street or along a perimeter block.

What will work:

- Harmonious street scene despite change in building typology
- Proportions aligned
- Colours, materials and tones are the same along the street
- Street trees help with transition and it may be appropriate for limited building setback along the street
- Roofline is designed to concentrate scale on opposite side of street
- Treatment of public realm is the same i.e. boundary wall

Storage and waste facilities



The storage and collection of refuse and recycling must be carefully considered so as not to visually detract from the overall appearance of the development. All residents must be provided with adequate internal and external storage and be able to dispose of household waste conveniently. The layout of the development must be designed to ensure that collection by refuse collection can be easy and efficient.

All houses and apartments must have adequate storage space for refuse and recycling containers within their own property boundary or shared space. The layout of bin storage areas should allow flexibility to adapt to new waste segregation and storage requirements given that waste policies are likely to change over the duration of the development and beyond. Such accommodation must have minimal impact on the street scene and, for houses, must be located either behind the building line or at the rear of the property.

Set back and boundary treatments

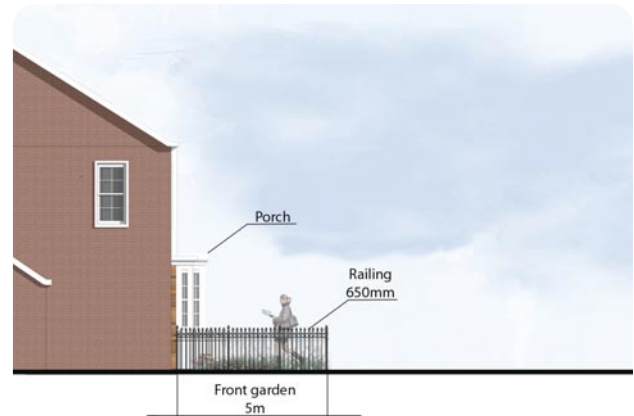
The set back of a building relates to the distance between buildings and the pavement and road. Setbacks often take the form of small front gardens and are important in helping define character of a street. Different setbacks should be used according to the street type being created. There should be an emphasis on landscaping as part of boundary treatments recognising the rural nature of the area and the need to encourage biodiversity.

Continuity of boundary treatments will be required along the length of streets on public spaces within particular character areas. An agreed palette of materials and details for boundary treatments must integrate with the overall palette of materials and public realm/open space proposals.

Design options front garden

A front garden should be situated between the plot line and the building edge. The garden can be delineated by a boundary treatment of between 450mm and 1000mm. The front garden for larger plots should have a depth of 5-10m. Planting along the property line is encouraged.

Front garden



Bay windows or porches may project into the garden. A boundary railing or wall of brick/stone or render may be between 650-1000mm with planting on the plot boundary line to reinforce it. The front garden should have a depth of between 5-10m. Where the fencing interferes with a highway visibility splay, it may be necessary to reduce the height or demonstrate that the fencing is of an open design which permits adequate views to be gained through it.

Narrow front garden



The plot line should be delineated by a railing or wall up to 1000mm high. The front garden should have a depth of between 1.5-4m and should be planted with hedging. This boundary height should be no more than 600mm in locations close to where children are likely to be playing or walking.

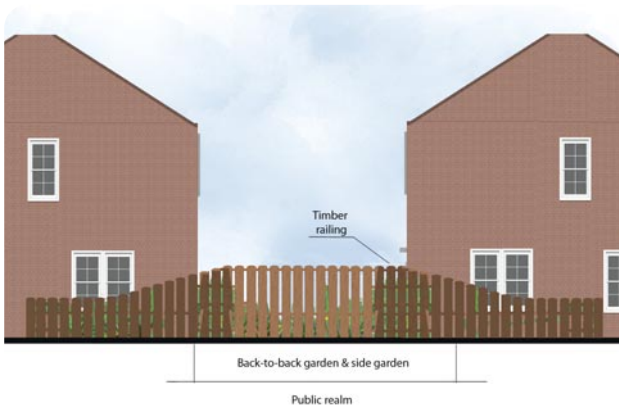
Front strip



A narrow strip of land should separate the building edge from the plotline. This should allow for an adequate amount of soil to encourage plant growth. A low level permeable boundary may be incorporated. The front strip should have a depth of between 0.6-1m and be either delineated from the pavement by using higher quality materials or planted with low shrubs or climbers.

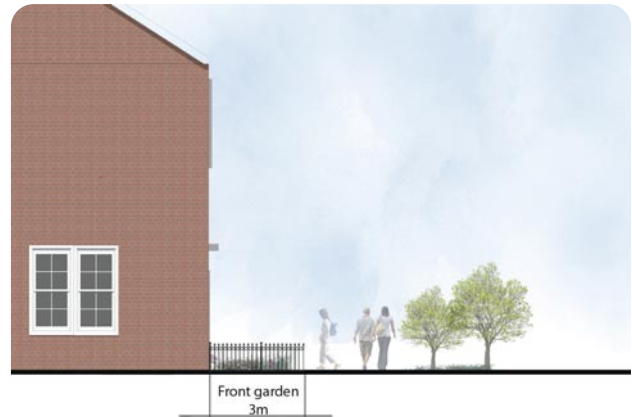
It is important to ensure there is an adequate amount of soil to allow planting to grow and remain healthy.

Side boundaries



The inclusion of perimeter block development will lead to the predominance of back-to-back gardens and corner properties with side gardens bounding the public realm. Where gardens side onto the public realm a stepped approach should be sought with the provision of a brick and timber/railing combination (see illustration).

Open space edge



The treatment of the edge is of great importance and must be carefully designed in relation to the characteristics of the landscape immediately adjoining. Residential properties should provide low level permeable boundary treatment.

Rear boundaries

As discussed in the layout principle and parameters section the rear of buildings, rear gardens or high level boundary treatment should not front onto green infrastructure such as SSAANGs and hedgerows, or the public realm.

9.4 Facilities

Section 7.4 highlights the need to create a truly sustainable thriving community in SSGA through the provision of a neighbourhood centre and where necessary additional small retail provision in the form of local parades. The below guidance has been provided to ensure that facilities provision is designed to a high standard and integrated into the proposed and existing communities Neighbourhood Centre and local parades.

Neighbourhood centre/ commercial parking

In line with the objectives of this SPD a neighbourhood centre will be provided on the Land North of Burdon Lane site. The centre will form a central hub for the SSGA and ensure a sustainable neighbourhood is created.

The neighbourhood centre will comprise of:

- Retail provision
- Primary school
- Wheeled Sports Area
- Formal play space
- Multi User Games Area
- 3G pitch
- Parking facilities capable of serving all of the above and the SSAANG
- To be served by bus service

The neighbourhood centre will be accessible, it being located on the primary bus route, Nettles Lane multi-user route and the pathways within the neighbouring SSAANG. Green links provided throughout the sub area will connect to the neighbourhood centre providing increased movement between the residential areas, the neighbourhood centre and SSAANG.

Properties located around the neighbourhood centre will be of the highest density within the SSGA up to 45dph.

Each sub area may include small retail or facilities provision in the form of local parades, dependent on the needs of the area. These should be provided in locations where a focal point will be created, for example neighbouring play areas and public amenity space. As the local parades may be utilised by existing residential areas neighbouring the SSGA their locations should consider the connections into the existing communities.

To ensure the neighbourhood centre and local parades are fit for purpose, proposals should satisfy the following simple overarching design principles.

- A strong physical and visual relationship between the public realm and building frontage must be maintained through providing activation onto the public realm through the positioning of shop windows and entrances
- Service areas are to be located in discrete locations and screened from public view. These should not be located in areas bounded by the public realm
- Strong architectural design, the neighbourhood centre will be a focal point for the SSGA and the built form should reflect this



Provide high quality public realm and landscaping which is constructed of durable and easily maintained materials.

Adequate parking will be required for the facilities provided within the neighbourhood centre, it is not expected for this to comprise of one large car park which would be overly dominant on the landscape. Parking for the school, play pitches and SSAANG can be provided together and parking to serve the commercial facilities separately (this will be dependent on the location of the facilities within the neighbourhood centre).

Materials proposed for parking within the neighbourhood centre and local parades will need to be high quality and durable which does not detract from the public realm. Areas of soft landscaping should also be included to reduce its dominance on the street scene.



School

To ensure designs for school facilities create stimulating and creative environments and encourage enjoyment the following design principles should inform development proposals;

- School facilities should relate visually and physically and in terms of their scale and massing to their surrounding neighbourhood, maximising the opportunity to exploit the natural surveillance that residential properties in particular offer. This is especially the case given the aspiration that most children will be encouraged to walk to school;
- Opportunities to share school resources with the community such as the 1 x 3G Pitch 1 x Multi Purpose Pitch, should be explored to further integrate the school with the local neighbourhood - thereby creating a sense of 'community ownership' and developing a 'continuity of occupation';
- Relationships with safe pedestrian access and cycle routes to schools needs to be carefully considered in considering the position, layout and entrance/egress points to the school;
- Physical provision should be made for parents delivering and picking up children from school. This facility should be designed as an integral part of the school complex and plays an important role in encouraging a greater sense of community;
- The requirements of school safety issues need sensitive and creative interpretation.

9.5 Green Infrastructure

The aim of the masterplan is to create an outstanding natural living environment. This will be developed through providing a series of neighbourhoods set within a green and leafy urban fringe environment which enhances the existing natural features of the area, emphasises local distinctiveness, protects habitats and encourages outdoor activity.

As highlighted within Chapters 4 and 7 SSGA benefits from a unique landscape setting, with a number of protected wildlife sites, mature woodland, street trees (Cherry Knowle), hedge rows and varying topography. The protection and enhancement of the natural environment including habitats and species is a fundamental requirement of the SSGA SPD. In addition, regular contact with the natural environment is not only beneficial to wildlife but to human health as well, including mental health and well-being.



Multi-functional green spaces

A network of multi-functional green spaces will be created to provide both ecological and community benefits. The greens spaces will include, but not limited to:

- Amenity green space
- Country Parks (SSAANGs)
- Allotments
- Play spaces both formal and informal
- Landscaping

Existing hedge rows and woodlands will remain and form the basis of development proposals. Gaps in existing natural features such as hedge rows can be utilised to provide access and services.

The green spaces will be connected via green links which utilise hedge rows and natural heritage assets buffer zones (see 7.1) to create a recreational movement network linking all open spaces throughout

the SSGA. Green links will aid in habitat creation and act as wildlife corridors. The routes will provide both pedestrian and cycle movement and consist of:

- Aggregate surface materials
- Down lighting
- Areas for natural play should be provided along the routes.

Design of green spaces

Green spaces will be designed to encourage social interaction and form useable spaces which have a defined function and include areas for activity and informal play, areas of incidental open space which have no defined purpose or function should be avoided. The interface between the built form and open and natural space will need to be addressed in a positive way to ensure spaces are overlooked, safe and well lit.

Consideration of the views into and out of green spaces will need to be considered to encourage their use and have a positive visual impact on users and surrounding residents.

The landscape and open space proposals within developments should be carefully designed to build up the existing natural features of the sites to create an attractive and sustainable development which is accessible by all.



Properties will be required to provide activation and frontage over routes and open spaces to provide natural surveillance and a safe environment.

Green spaces and areas of the public realm must be designed to be accessible to all, including those with mobility impairments and parents with children. There may be reasons such as topography/levels where it is



Fig 24 Green Infrastructure

not possible to achieve this. To ensure proper provision, care and sensitivity is required in the design of open spaces, in particular, gradients/inclines, pedestrian and cycle crossings and the type of materials, street furniture and planting used within the public realm. Proposals must:

- Integrate the adjacent natural environment into development sites to create substantial green corridors
- Avoid unnecessary street clutter or highway signage.

SSAANGs



SSAANGs will have a country park feel, designed to a high quality to attract residents of the existing and new community. The parks will include:

- Meandering recreational routes constructed of natural materials which blend into the landscape
- Meadows grass to encourage wild flowers and habitat creation
- Scattered tree planting
- Soft landscaping
- Water features
- Way finding
- Natural play facilities
- Seating areas – where possible using natural materials
- Lighting – down lighting.

Further details on SSAANGs can be found in Appendix 3.

Planting



The use of trees within the public realm is important in establishing a coherent streetscape and emphasising changes in character. Native large trees such as Oak and Lime should be used in key spaces where they will have space to grow to full maturity. Species such as Plane and Maple are suitable urban street trees. It is important to use a range of species on an individual site to establish diversity that has the potential to withstand new diseases that may develop among particular tree species.

Areas of soft landscape should be simple in design comprising grass (mown, long and meadow), hedging and specimen trees. Bulbs, shrubs and herbaceous perennial planting should be used to create colourful and attractive spaces, route ways and entrances.

Low maintenance planting or wildflower meadow may be a more appropriate solution than mown grass where access is difficult, for example on roundabouts.

All planting should be in accordance with the National Plant Specification and current British Standards.

Planting within SSAANGs should be in accordance with guidance set out in Appendix 3

Natural play

Play provision for children and young people should be a key component within development proposals. As well as formal play, natural play should be considered, ideally with direct links to the countryside to provide children with a rich natural environment to explore and enjoy. Natural Play uses materials such as fallen trees and undulating surfaces to provide stimulating landscapes for play. Proposals should consider the following:

- Areas of play space should be located in nodal locations off the main primary and secondary streets.
- Where possible, play spaces should be integrated into open spaces, public squares or pocket parks

with the aim of creating high quality play areas, within open spaces that provide a range of recreational opportunities.

- Encourage 'Play on the way' as a design concept and should include a series of formal play equipment and interlinked play equipment along key routes.
- Provide a diverse range of safe, attractive and stimulating spaces throughout the development, easily accessible and catering for all age groups
- Consideration needs to be given to the orientation of the built form adjacent to these places to ensure safety of children.
- A simple palette of natural materials (such as timber) and muted colours to be used throughout this area for play equipment and street furniture, such as seating.



The Forestry Commission and Play England have provided guidance and ideas on the type of natural play facilities which can be offered within development.

Public art



Artists are uniquely placed to bring a sense of originality and delight to the creation of new places. Artists have a key role in the creation of a thriving and distinct community by delivering projects that make direct connections with values and identity of the people who live there, both existing and those to come in the future.

The natural environment should be a major source of ideas for place making and cultural development. Where there are opportunities or a requirement for public art, proposals should consider the integration of public art throughout the design process.

10 Indicative Masterplan (sub-areas)

10.1 Introduction

An Indicative Masterplan has been produced to further supplement the concept masterplan and design code. The following Site Wide Character Areas and Sub Area Design Coding have been developed to provide further clarity on the expectations for the development of the area.

The Indicative Masterplan is not prescriptive in limiting how the site must be developed, nor is it exhaustive in suggesting that this is the only way the site can be redeveloped. It is an interpretation of how the council's aspirations for the area can be accommodated within the physical constraints identified in this document.

Other interpretations of how the site could be taken forward to meet the objectives of the masterplan may be appropriate and the council will be open to discussions with any interested parties who wish to provide their own interpretation of the masterplan.

However, any proposals which compromise the viability, deliverability and quality of any part of the masterplan will be resisted.

The following sections outline the coding elements used to define the four sub areas within SSGA. The essential characteristic of each sub area is explained at the beginning of each section. Each area is intended to have a change in ambience, appearance and urban form. At the same time the sub areas must combine to create a unified whole; this juxtaposition is fundamental to the overall unity of the development. Getting this balance right is crucial to place making.

10.2 Site wide character areas

There are several elements and physical features which relate to all of the sub areas within the SSGA. To ensure the design of these locations relates across the SSGA four site wide character areas have been produced.

Ryhope Doxford Link Road (RDLR)

This character area follows the route of the RDLR.

- Due to the length of the character area it is expected that a variety of building heights and house type's are included to provide visual interest to the streetscene along the RDLR and break up the overall mass of the developments properties will front and side onto the link road.
- Density of development between 30-35 dph.
- Consistent building line provided throughout the length of the character area.
- The landscape belt of approx. 15-20m will provide a pedestrian/cycle routes, low level native species planting and scattered tree planting.
- Private accesses and parking are not permitted directly from the RDLR therefore shared surface areas constructed of high quality surface materials will be required, these can be located within the landscape belt.

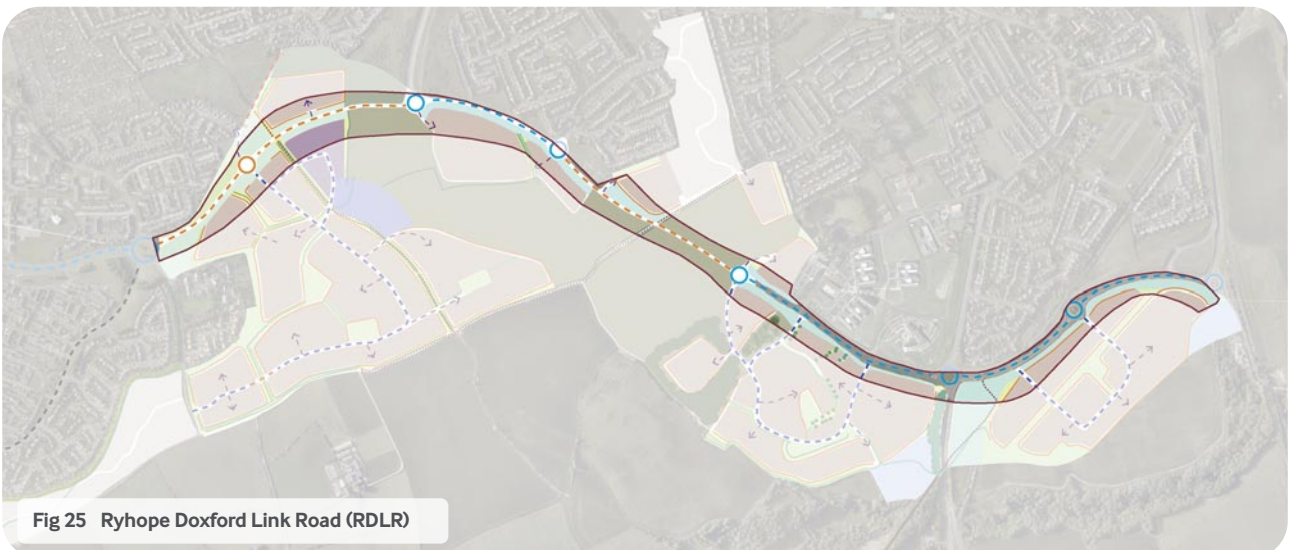


Fig 25 Ryhope Doxford Link Road (RDLR)



Fig 26 Urban-Rural Edge (URE)

Urban-Rural Edge (URE)

The proposals aim to provide a physical and visual link from the built environment into the rural landscape through creating a semi-rural feel.

- Lower density housing of 15-20dph.
- Detached larger family dwellings .
- Range of building heights from bungalows to two storey.
- Properties to have a minimum 5m front garden to include an area of landscaped amenity space and tree planting.
- Streets will be shared surface areas which function as a high quality public realm which blends into the natural landscape and act as a visual link between the urban and rural environment.
- Parking to be provided in plot through garages or driveways, where this is not achievable parking spaces within the shared surface will need to be design so as not to comprise the visual quality of the public realm. Due to the property types being large, two parking spaces per property should be provided.

- Soft landscaping planting should enhance the natural features bordering the area. Species choice should take into account biodiversity, the use of native species, form, colour, texture, reliability and resistance to disease.
- Establish strong physical, functional and visual connections between open space and the built form.

Urban Edge (UE)

These are areas where new development borders the existing urban area and it is crucial that they integrate well with existing communities

- Densities to reflect the existing residential neighbourhoods.
- Building heights between 2 -2 ½ storeys (however, careful consideration should be given to site topography)
- Provide adequate connections through to existing communities where possible.
- New development blocks located on existing highway will be required to provide properties which front onto the highway to provide a positive streetscape and activation



Fig 27 Urban Edge (UE)



Fig 28 Urban (U)

Urban (U)

These are areas that follow primary streets

- 4m set back from front gardens (see primary street illustration)
- High density housing of 30-35dph
- Semi-detached and linked properties
- Building heights of 2-2 ½ storeys
- Tree lined street through planting in residential properties front amenity space.
- Provide continuous frontage to the streetscene.
- Parking to be combination of in plot and on street within the parking verge (Movement and Streets section). The inclusion of garages will be acceptable form of on plot parking however the garage must not dominate the street scene. The inclusion of a high proportion of integrated garages will not be considered acceptable.

10.3 Chapelgarth

Chapelgarth is the most enclosed sub area of the SSGA, due to the valued natural features of the site with mature ancient semi-natural woodlands and hedge rows. New development within the site will create a feeling of a community set within the rural fringe of the city. The topography of the site allows for extensive views of the city which should be retained and enhanced through new development.

Street Network

Vehicle network (Fig 29)

1. The primary access route will run through the centre of the site and 'loop' between the two existing roundabouts on Weymouth Road.
2. Utilise gaps in existing hedge rows to provide access routes throughout the area.
3. It is anticipated that individual access will be provided to this section from Weymouth Road in the form of a secondary street.
 - a) Pedestrian connections and an emergency vehicle only access will be provided into the remainder of the subarea.
4. Access from Moorside Road or the roundabout (Moorside Road/Weymouth Road) will not be encouraged due to existing vehicle levels and the removal of mature woodland on the north western boundary.

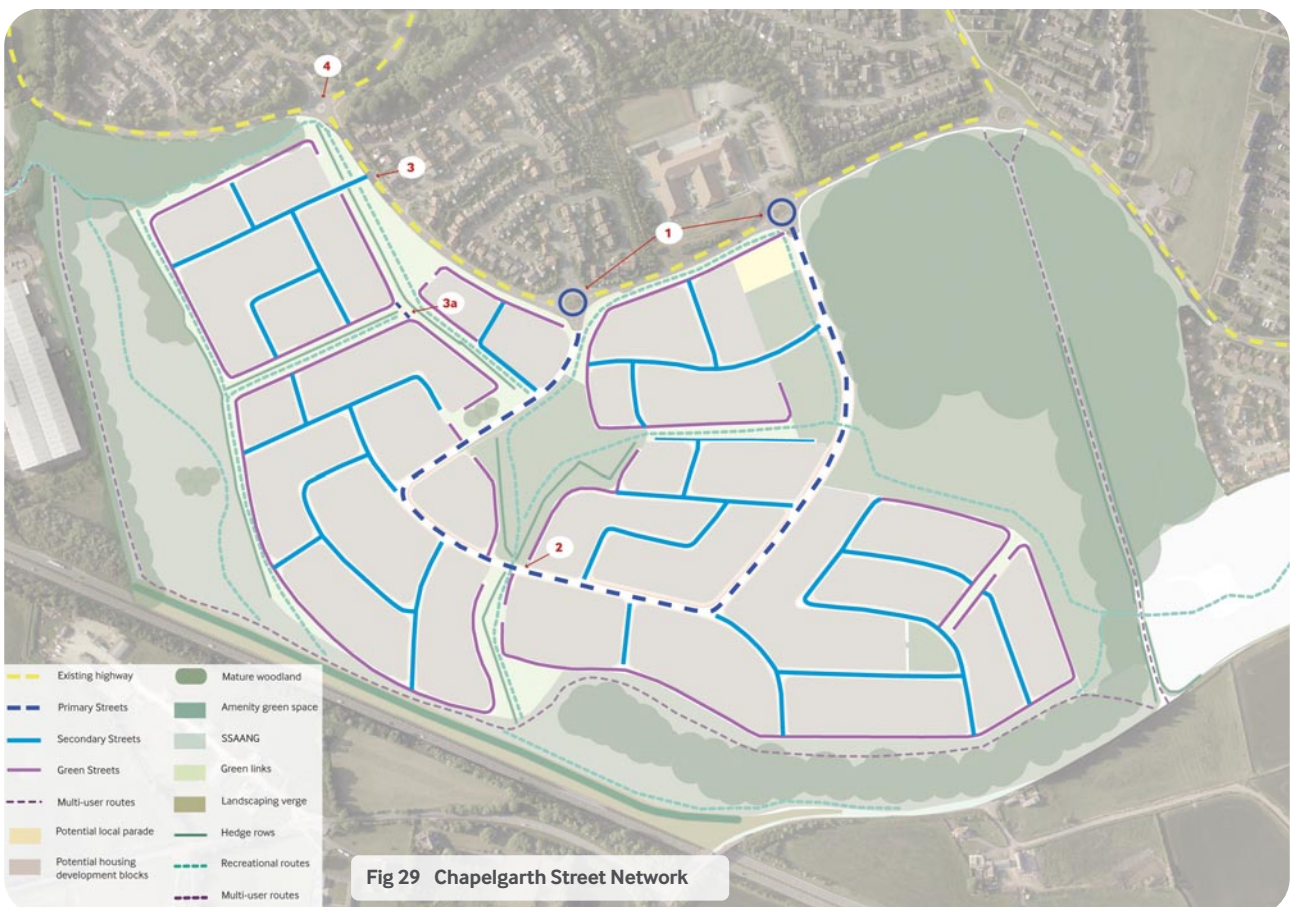




Fig 30 Chapelgarth Green Infrastructure and Recreation Networks

Green infrastructure (Fig 30)

1. Landscaping within proposals will add to the provision of woodland and mature trees, using native species to complement those species found within Blakeney Woods LWS.
2. A SSAANG will be located on the Western edge of the site in the ready made green space surrounded by tree planting and hedge rows.
 - a) The area will provide improved pedestrian and cycle connections to Doxford Park District Centre.
 - b) Details of the design of the SSAANG are included in Appendix 3.
3. Due to the protected nature of Blakeney Woods LWS a buffer will be provided within which no development other than necessary vehicle/pedestrian connections will be provided.
 - a) The area will include an extension of Blakeney Woods southward to create a woodland connection with the tree belt to the south.
 - b) Allotment provision may also be located within this buffer.

4. Central open space is to be provided utilising the area formed through hedge rows and tree planting. The open space will include:
Play area, amenity open space and a formal pond which will also act as a SUD.
5. Properties to front onto the tree belt on the southern edge of the site and follow the design principles set out within the urban-rural edge character area.
6. A buffer of approx. 15m will be provided for blocks located on Weymouth Road, within this buffer pedestrian and vehicle movements can be located on a shared surface access.

Pedestrian and cycle recreational network

7. The existing multi user route through Blakeney woods LWS will be retained and enhanced. A link through the hedge row onto the route should be provided.
8. Pedestrian and cycle connections into the neighbouring Burdon Lane development will need to be provided to ensure the SSGA is an integrated and well-connected neighbourhood.
9. Green links will utilise the existing hedge rows and mature tree planting buffers to provide an inter-

connected network of recreational routes linking the SSAANG, central green space and Blackney woods.

10. An existing pedestrian and cycle link is located on the southern boundary of the area forming part of the W2W cycle route. In order to create a safe and welcoming cycle/pedestrian route which is overlooked the current cycle route will be relocated to the north of the hedge row with development facing onto it. It is then anticipated that the old cycle route is replaced with further planting to act as noise attenuation from the A19.

Built form (Fig 31)

1. Access will not be provided directly from Weymouth Road therefore properties will be expected to provide a combination of front and side elevations onto the highway.
2. It is anticipated that a small retail provision in form of a local parade will be provided within Chapelgarth, the indicative masterplan has located this on Weymouth Road to serve both the new and existing communities.
3. Development blocks will be required to respond to the topography of the site and optimise views toward the north east over Blakeney Woods LWS to the city and eastward toward the coast.

4. Unique rural feel in this location in comparison to the rest of the Chapelgarth site due to the hedge rows and mature woodland enclosing the area. This should be emphasised through creating a distinctive development with a unique character.
5. Gateways have been identified on the plan below and should include landmark buildings and an alternative approach to architectural design.
 - a) Weymouth Road roundabouts are the primary entrance into the site and the built form will need to reinforce this through the scale and massing of the buildings and strong architectural detailing. The use of public art will also be welcomed in these locations.
 - b) Gateway to the site via pedestrian and cycle connections from the SSAANG. This gateway will consist of a more green entrance to Chapelgarth through landscaping with the surrounding residential development enhancing this, it is not expected for increased building heights in this location.
6. Properties which will be subject to long distant views, adjacent Weymouth Road and in key locations will need to provide strong architectural detailing.





10.4 Land North of Burdon Lane (LNBL)

LNBL is the largest of the four sub areas and consists of expansive open fields with limited enclosure and a sloping topography. New development will work with the contours of the land and provide varied character areas which create a sense of community. Development will also avoid the highest points of the site to reduce its visual impact.

Street Network (Fig 32)

1. Access to this section to be provided from Burdon road however, this will not provide private vehicle links into the rest of the LnoBL development and will serve up to 100 units, unless agreed otherwise
 - a) A bus only link will be provided through to the larger site area.
2. Primary vehicle route to provide a looped connection to the local centre
3. Access to the developable land to the east of the country park will be provided from the two existing roundabouts on Eltham Road,
 - a) A roundabout will be provided to enable connections to the existing residential streets and the RDLR.
 - b) An area of greenspace will be created between the existing residential areas and the RDLR.
4. Development to the west of the residential development at Bevan Avenue will be expected to provide both vehicle and pedestrian connections through to the development currently under construction creating an integrated neighbourhood
5. It is anticipated that the RDLR will connect between the Doxford Park/B1286 and Eltham Road roundabouts. The route will use the existing B1286 junction however it will divert from the existing carrieway route.

Green Infrastructure (Fig 33)

1. Landscaping and amenity green space to be provided between existing residential areas and the new development, a minimum of 10m will be provided. (Careful consideration should be given to the difference in levels between existing residential properties and the amenity green space so as to avoid overlooking).
2. Amenity green spaces will be provided throughout the site and include areas for natural play and be interlinked through green links.
3. A SSAANG will be provided within the site and will flow into the Cherry Knowle sites provision to create a large country park.
4. Development which directly borders the country park (SSAANG) will be required to provide frontage and activation onto the green space whilst also following the design principles of urban-rural edge character area.

Pedestrian and cycle recreational network

5. Nettles lane will be upgraded to a mutli-user route, residential development neighbouring this will need to provide activation and frontage onto the route.

6. Burdon Lane to be stopped up and converted into a multi-user route., once the RDLR is complete and operational At this point the green route running to the north of the hedge row will divert onto the multi-user route.
7. Green links will utilise the existing hedge rows and mature tree planting buffers to provide an inter-connected network of recreational routes creating a continuous green route from the land to the west through the site and into Cherry Knowle. The green links will connect with amenity green spaces the neighbourhood centre and the SSAANG.





Fig 34 Built form

Built form (Fig 34)

1. Separated from the central area of LNoBL by Nettles Lane the area leads toward the central country park and the creation of a semi-rural character will be expected. This will be established through providing low density housing.
2. Only development to occur north of the RDLR therefore an individual character is expected with densities to represent the existing residential development to the north.
 - a) Development layout will maximise views south over the SSSA.
 - b) Due to the elevated position of this section the built form will be subject to long distant views, therefore high architectural quality and a varied roofscape will need to be provided.
3. Densities will need to relate to the existing urban development to the north. The separated nature of this location and the topography creating a bowl effect will lead to a development with an individual character.

4. Densities to decrease toward the urban boundary and the SSAANG and properties within these locations to follow the principles set out within the Urban-rural edge character area.



5. Properties neighbouring the neighbourhood centre should be of a higher density and follow the design principles set out within the Urban character area.
6. Development to connect and thoroughly integrate with the existing residential area at Bevan Avenue. Through following the existing street pattern and providing back to back development.



8. Due to the sloping topography of the site several areas will be subject to long distant views, including those bordering the Country Park, the RDLR and the most northerly development blocks. These properties will need to provide strong architectural detailing and variety.

7. Development bordering Burdon Lane and Nettles Lane will be required to provide frontage onto the multi-user routes to ensure adequate natural surveillance.
 - a) Gateways have been identified on the plan below. And should include landmark buildings and alternative approaches to architectural design.
 - b) Proposed roundabout on the RDLR will be the primary entrance into the site and the built form will need to reinforce this through the scale and massing of the buildings and strong architectural detailing. The use of public art will also be welcomed in these locations.
 - c) Gateway to the site via pedestrian and cycle connections from the Country Park (SSAANG). This gateway will consist of a more green entrance to LNoBL through landscaping with the surrounding residential development enhancing this. It is not expected for increased building heights in this location.
 - d) The gateway into the area of Burdon Lane converted into a multi-user route will be important to positively announce the arrival into the country park and create a welcoming first impression to encourage the routes use.





10.5 Cherry Knowle

Cherry Knowle consists of a previously developed site with sloping topography which gradually steps from west to east. Cherry Knowle Dene LWS is also located on the southern boundary and mature woodland can be found in areas of the site. New development will maximise potential views toward the coast and Ryhope Pumping Station. New development will also enhance the woodland setting and elements of the sites previous developed nature such as tree lined streets.

Street network (Fig 35)

1. For the main section of the development site the primary street will be accessed via a junction on the existing section of the RDLR opposite Hopewood Park, the street will then run through the centre of the site and connect to the roundabout on the edge of the existing RDLR.
 - a) A second primary street may connect to the RDLR opposite Waterworks road junction and act as a bus route (see section 9.2).
2. For the developable area to the north of the RDLR access will be taken from the existing roundabout, however this should be designed as a secondary street as a bus route will not be provided in this section.
3. The developable area to the east of the SSAANG and south of Burdon Lane will be accessed via Burdon Lane.
 - a) Vehicle connections will not provide direct access onto the RDLR.
 - b) Properties which are located on the northern edge of this section will not be directly accessed via Burdon Lane it is therefore important that the properties provide front and side activation to provide natural surveillance onto Burdon Lane.
4. Access for the developable area situated within a mature woodland should be provided through a break in the tree line, it is not envisaged for there to be more than one entrance and exit.
5. The highway surrounding the green space should not consist of a standard tarmac surface, it should be considered as an extension to the green amenity space through providing a shared surface which functions as a high quality public realm where pedestrians have priority and vehicles do not dominate the streetscene

Green infrastructure (Fig 36)

1. Green links will be provided throughout Cherry Knowle to provide connections from the country park (SSAANG) located to the north west of the site to the amenity green spaces within the site and then through to the South Ryhope site.
2. Due to the previously developed nature of the site many of the existing trees within Cherry Knowle are protected under a Tree Preservation Order (TPOs) it is therefore important the layout of new development takes these into consideration and utilises them as green links.
3. An existing hedgerow is located to the south of the site and will be retained within development proposals therefore an amenity green space has been formed around it.
4. A SUD has been developed on the south eastern corner of the site as flood mitigation for the Hopewood hospital, it is anticipated that a new SUD for the development will be located with the neighbouring vicinity.
5. An orchard will be provided to further enhance the existing mature tree belt.

Built Form (Fig 37)

1. A unique section within the Cherry Knowle site due to its setting within a mature woodland, therefore the development of an individual character area will be expected to maximise the natural setting.
 - a) Properties will face onto the tree belt and its related buffer zone.
 - b) A standard highway would be an inappropriate response in this location therefore green streets will be provided. Vehicle speed reduction measures on the perimeter of development bordering the woodland may also be appropriate.
 - c) A rural character to the development should be created through providing lower density housing.
2. Specific accommodation for older people/retirement village will be provided within Cherry Knowle and will require the following principles:
 - a) Consist of either an apartment complex on its own or in combination with individual bungalows.
 - b) To include community and retail facilities which will be available to the wider community.





Fig 37 Built form

- c) Should be located on the Primary Street to provide easy access to public transport.
- d) To be no more than 3 storeys in height as an apartment complex
- e) Situating the older people’s accommodation to neighbour the RDLR will also add further variety to that character area.
- f) Green space to the west can be utilised by residents as well as the green route connection.
- 3. A set back from the boundary of the listed structures will be provided, the setback will be required to be sufficient to allow for a viewing ‘tunnel’ from the SSAANG to the pumping station and will include low level landscaping of native species planting.
- 4. Architectural style will make a positive contribution to the significance and setting of the heritage asset through including elements which relate to the SAM and provide a strong character.
 - a) The height of properties must not interrupt key long distant and short views of the heritage asset
- 5. ‘Garden Lodge’ of the former Cherry Knowle hospital site is not included within the red line boundary of the site and has been developed as a car park for the

Hopewood Hospital. The development blocks within this location will therefore been designed for residential units to back and side onto this.

- 6. Urban-rural edge character area design principles to be followed for properties on the southern edge.



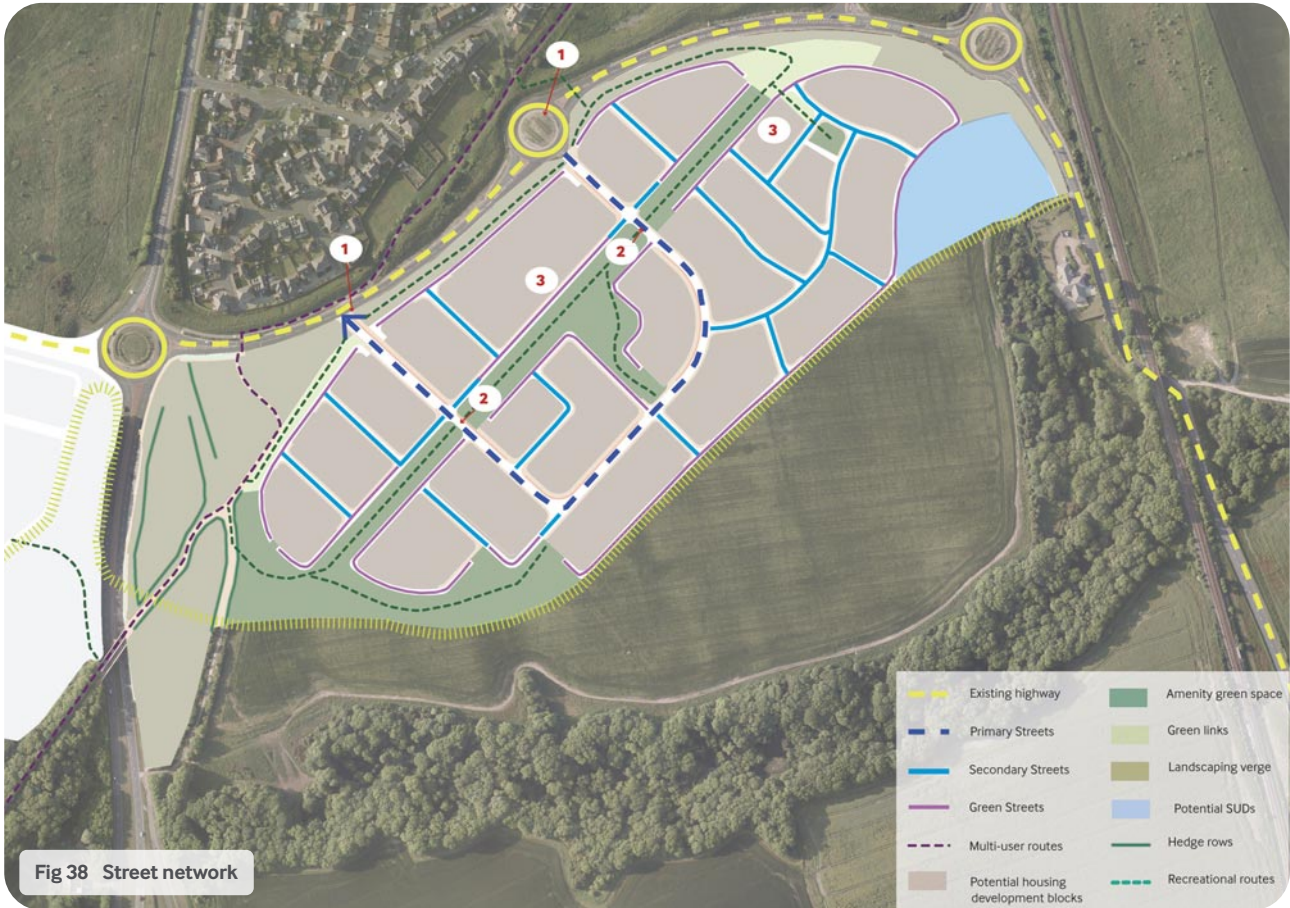
- 7. Densities to decrease towards boundary of the site to increase the ‘feel’ of countryside surrounding site.
- 8. Opportunity to maximise east facing properties with views to the coast within a woodland setting.

9. Orientation of development blocks will also create north west facing properties to optimise views of the pumping station chimney.
10. Properties on the southern edge will be subject to excellent countryside views, properties should follow the design principles for Urban-Rural Edge character area.
11. The area is to be characterised by a higher density of properties with a more suburban feel which highlights the green as a focal point of the location.
 - a) Provide adequate activation and increase the sense of ownership for the public space.
 - b) Properties surrounding the green space should include a small front garden to demark the separation between public and private space.



12. Separated from the rest of Cherry Knowle Sub-area by the SSAANG and accessed from Burdon Lane the development should consist of low density large family dwellings
 - a) Subject to long distant views, architecture will need to be of a high standard and include contemporary features.
 - b) A landscaped green space will be provided between the existing properties to the east of Esdale Road and the new development.
 - c) Properties will be required to provide activation onto the SSAANG and provide an appropriate design response to create a gradual visual and physical link into the green space. This can be provided through following the design principles set out within the Urban-Rural Edge character area.
13. Gateways have been identified on the plan below and should include landmark buildings and alternative approach to architectural design.

- a) RDLR roundabouts are the primary entrance into the site and the built form will need to reinforce this through the scale and massing of the buildings and strong architectural detailing. The use of public art will also be welcomed in these locations.
 - b) Gateway to the site via pedestrian and cycle connections from the SSAANG and the National Cycle Route 1 to the east. This gateway will consist of a green entrance to Cherry Knowle through high quality landscaping with the surrounding residential development enhancing this, it is not expected for increased building heights in these locations.
14. Properties which will be subject to long distant views; surrounding the country park, adjacent the SUD and in key locations including along the RDLR will need to provide strong architectural detailing and variety.



10.6 South Ryhope

South Ryhope is situated at the lowest point within the SSGA and in the most open location with limited existing features to the site. It is bounded by the Green Belt to the south and the coast to the East. Development should provide a design response appropriate to its setting and surrounding context whilst creating the feel of a coastal community.

Street Network (Fig 38)

1. The Primary Street will be accessed via the easterly roundabout on the RDLR, however a secondary exit only route can be provided between the two roundabouts on the RDLR
2. The Primary Street will dissect the amenity green space. In these locations adequate pedestrian and cycle crossings will be provided.
3. Due to the location of the Green Spine running through the centre of the site it will be surrounded by vehicle streets therefore where possible the adjacent highway should consist of a green street. Where this is not feasible measures should be put in place to reduce vehicle speed to ensure a safe environment.



Green Infrastructure (Fig 39)

1. Regional high pressure gas main runs through the centre of the site from the south west to the north east. Development cannot occur over the pipeline and an easement will be sought. It is therefore proposed that a linear residential amenity park is provided within the no build zone.
 - a) This 'green spine' will link up with the national cycle route and to a pedestrian crossing into Ryhope.
 - b) Provide play facilities.
 - c) Create a safe and useable space.
2. A landscaping belt will be provided to the RDLR and will follow the RDLR character area design principles.
3. To prevent access into the protected Ryhope Dene LWS a permanent swale and hedge row barrier will be created. The bund will run the length of the southern development edge.



Fig 39 Green Infrastructure



Fig 40 Built form

Built form (Fig 40)

1. Properties will face onto the 'green spine' to provide activation and a sense of ownership.
 - a) Due to the length of the spine a variety of house types, building heights and materials will be required to provide interest and an 'enlivened' streetscape.
2. At the highest point of South Ryhope the layout of development will be designed to maximise coast views through providing east facing development blocks. The northern elements of these blocks will also be subject to long distant views to Ryhope Pumping Station Chimney.
3. Properties to face onto National Cycle Route 1 and areas of amenity space
4. Properties to the south should also maximise views to County Durham
5. Density should gradually lower as approaches rural edge of site from 35dph to 15dph.
6. Development blocks which border the southern edge of the site will provide frontage onto the Green Belt/Bund.
7. Due to the grid form of development the site will include several long distance views; the termination point of these will include landmark buildings to aid in navigation.
8. The properties which border the RDLR and Southern Radial Route will be required to provide strong architectural detailing and variety.
9. Gateways have been identified on the plan below and should include landmark buildings and alternative approach to architectural design.
 - a) RDLR junctions provide the primary entrance/exit for the site and the built form will need to reinforce this through the scale and massing of the buildings and strong architectural detailing. The use of public art will also be welcomed in these locations.
 - b) Gateway to the site via pedestrian and cycle connections from the National Cycle Route 1 to the west will consist of a green entrance to South Ryhope through high quality landscaping with the surrounding residential development enhancing this.

11 Delivering the vision

11.1 Introduction

The success of the SPD/framework and fundamentally SSGA will ultimately be judged by what is delivered and will depend to a large extent on the continued partnership working of the landowners, the Council and other key stakeholders to secure delivery of a high quality and sustainable place and supporting infrastructure in a timely way. It is essential that development is brought forward in a coordinated and cohesive manner with the bigger picture clearly in mind, in this way the overall vision and objectives for the area can be achieved.

11.2 Amount and use

The following provides a summary of the estimated figures for development/land use at the four sub-areas.

11.3 Housing delivery

The Strategic Housing Land Availability Assessment (SHLAA) sets out a year on year trajectory for the

provision of housing in the city and anticipates the delivery of approximately 30-35 dwellings per annum, per site. It is anticipated that it will be a similar delivery rate for developments within SSGA, although this is expected to be exceeded where there is more than one house builder on site at any one time.

11.4 Phasing

Planning permission is in place for three of the four development sites of the SSGA..

- Chapelgarth has outline planning permission for up to 750 units, of which 160 of these have full planning permission. Works have started on this site with a number of dwellings already complete. It is anticipated that the full site will be built out by around 2035.
- Cherryknowle, outline consent in place for 496 dwellings and full planning in place for 304 dwellings. Works has commenced on site and a number of dwellings are now complete. It is anticipated that the full site will be complete by 2035.

Table 7 Estimated development/land use figures

	Chapelgarth	Land North of Burdon Lane	Cherry Knowle	South Ryhope
Approximate number of dwellings	750	1000	800	450
Centres	Small provision (local store)	Neighbourhood Centre	Small provision (local store)	Small provision (local store)
School	N/A	1.5 Form Entry Primary School	N/A	N/A
Allotment	10 Allotment Plots (either on site or off site)	15 Allotment Plots (either on site or off site)	12 Allotment Plots (either on site or off site)	6 Allotment Plots (either on site or off site)
SSAANGs	12 ha	17 ha	14 ha	none
Play facilities	1 Equipped Play Facility	1 Equipped Play Facility 1 Wheeled Sports Facility	1 Equipped Play Facility	1 Equipped Play Facility
Sports facilities/pitches		1 x 3G pitch 1 x grass pitch		

- South Ryhope has outline consent in place for 450 dwellings. It is anticipated that this site will be complete by 2037.
- Land North of Burdon Lane, a planning application has been submitted for the development of the majority of the site. The anticipated overall completion date for the full site is around 2038.

11.5 Planning obligations

The council has considered that the most appropriate mechanism to deliver the required infrastructure associated with SSGA will be through the use of S106s/S278s and conditions.

In order to ensure that infrastructure is provided in a coordinated and timely manner and that individual applications do not prejudice the ability to provide infrastructure on other parts of SSGA, the Council have sought to ensure that the 4 key sites, Chapelgarth, Land North of Burdon Lane, Cherry Knowle and South Ryhope come forward as outline/full planning applications and all S106s are agreed at this stage of the plan process. No S106s will be agreed at the Reserved Matters stage of the planning process in the case of Outline applications.

In the council's view a fair and practical method of distributing most infrastructure/S106 costs is to apportion them on a per dwelling basis. Certain costs, however, are based on site specifics.

In cases where the council will be responsible for implementing infrastructure, the council will seek a contribution towards staffing costs. For example, in the case of SAMM mitigation where the council will be responsible for implementing and maintaining the mitigation measures in perpetuity, a fee will be charged.

11.6 Infrastructure delivery

The sequence of development and provision of supporting infrastructure set out in this SPD has been structured to provide appropriate flexibility over where and when development takes place. It seeks to ensure that the necessary infrastructure is provided in a timely and viable way in the interests of delivery of the visions and objectives in a co-ordinated and comprehensive manner.

As development is expected to take place over a period of at least 15-20 years, it is recognised that an element of flexibility is retained in order that development can respond to changing circumstances over time.

11.7 Infrastructure requirements

Below is a summary of the likely infrastructure requirements for the SSGA. These are discussed further in the SSGA SPD Infrastructure Delivery Study. Costs are based on current or recent prices. Costs will rise year on year, in line with inflation and will be indexed linked.

11.8 Long term maintenance/management

The on going management of public space and SUDs is essential in ensuring SSGA development achieves its vision as a highly sought after place to live.

Preference is for greenspace/SSAANGs/GI corridors/play facilities/sport pitches to be implemented by the developer and maintained for a period of a minimum of 12 months following practical completion. Following this the council will then be prepared to adopt the land, providing it meets the expected standard. A commuted sum for maintenance for a 20 year period, will be payable to the council.

Should the developer prefer to appoint a third party to manage the greenspace, rather than the council, through S106s/conditions/legal agreements the developer must be able to provide Sunderland City Council with confidence that the green space will be maintained in perpetuity to a standard agreed by the council.

Where the above is applicable, a comprehensive landscape management plan will be required. This plan should outline a set of mechanisms in order to ensure proper management and maintenance of open spaces for the future as well as identify the relevant bodies expected to be responsible for the spaces.

Any management and maintenance regime will be needed to ensure that the quality of greenspace delivered at the outset of the development is maintained over subsequent years. Good management and maintenance will amongst other things foster a sense of ownership amongst residents and instil community pride and help residents and visitors to perceive the SSGA as an attractive and safe environment

A management board should be established that includes residents and council representatives.

Infrastructure requirements		
Infrastructure	Requirement	Estimated Cost*
Affordable Housing	10% requirement based upon a 75% social rented and 25% intermediate	Unknown - To be implemented by the developer
Education	Contributions towards the extension of two primary schools. Contribution to the development of a new 1.5 form entry primary school. Contribution to the expansion of one secondary school	£2,855-3,095 per dwelling
Children's Play Provision	Preference for developer to provide on site fixed play and maintain in perpetuity	Unknown- To be implemented by the developer
Sport and Leisure	Contribution towards sport and recreation facility within the wider SSGA. Including 3G pitch, multi purpose pitch and wheeled sports area then provide a commuted sum for maintenance (20 years)	£855-927 per dwelling
Greenspace	Preference for developer to provide and maintain in perpetuity.	Cost dependant on greenspace proposal
Ecology (HRA)	Provision of well connected SSAANGs. Where this is not practical contributions will be sort towards SAMMs	SSAANG to be implemented by developer, maintenance cost dependant on SSAANG proposal SAMM £1,804-1,845 per dwelling
Ecology (HRA)	Enhancement, protection and maintenance of the designated sites	Cost specific to each site Chapelgarth – £320-327per dwelling LNBL – £311-337per dwelling Cherry Knowle – £349-378 per dwelling South Ryhope – £491-532 per dwelling
Sustainable Urban Drainage System	Developer to implement and provide a commuted sum for maintenance (20 years)	Dependant on proposal
Public Transport	Contributions will be required towards the pump priming of a bus link.	£316-343 per dwelling
Allotments	Model suggests 15 plots per 1,000 households, on an average size of between 250-300 square metres . Preference for developer to include on-site. If agreed off-site a financial contribution will be sought.	£85.50-93 per dwelling for off-site contribution
Art	To be provided on site by the developer	Unknown- To be implemented by the developer
Junction Improvements	Junction improvements as required (site by site)	Unknown- To be implemented by the developer
Road	RDLR- Contributions towards the completion of the 'missing link'	£1,847-2,002 per dwelling
Cycling & Pedestrian Links	To be provided on site by the developer	Unknown- To be implemented by the developer
*Costs will rise in line with inflation and will be indexed linked		

11.9 Viability

Paragraph 34 of the NPPF emphasises that development contributions should not undermine the deliverability of the development plan. Therefore, the requirements around infrastructure provision set out in this SPD have been subject to viability testing to ensure that development is deliverable as far as is possible given the council's aspirations for delivery of affordable homes and a highly sustainable development with the provision of all necessary infrastructure.

Given the long timescale for delivery of the site and potential for uncertainty about the market conditions over that period, it may be appropriate to re-test the viability of the scheme at later stages.

Where landowners/developers wish to re-test scheme viability, the council will require an open book approach on viability to be taken. This will include a statement detailing the viability of the project and what is proposed to be delivered supported with financial appraisals which will need to include all information and evidence used to in the appraisal. Information received will be reviewed by the City Council's professional advisors in line with current guidance.

The land value and developers return elements of the appraisal will be determined as what is sufficient to incentivise the land owner and developer to sell or develop for the proposed scheme.

The council's professional advisors will advise the Development Management team on the statement of viability and general viability of a scheme being developed on the site.

Should no agreement on the viability be reached between the council and the applicant then the option to appoint an independent expert to act jointly for the applicant and LPA at the expense of the applicant will be

available to provide a report on the viability of the project and advise whether it is financially viable to provide

some element of the infrastructure requirements. The outcome of the report will not be binding on either party but can be used as further evidence for the LPA to determine the outcome of the request.

Where it is determined and agreed that viability is an issue, but some element of contribution can be made towards S106s, although not in full, it will be for the council to determine which infrastructure it deems to be a priority.

11.10 Compulsory Purchase Order

It is not considered that compulsory purchase orders (CPO) will be necessary for development to come forward at SSGA or in order to achieve the ambitions set out within the SPD, however, as a last resort, the council will consider the use of CPO powers where appropriate to secure the proper phasing and building out of the area.

11.11 Infrastructure commercial services

The Infrastructure Commercial Services (ICS) Section within SCC offers design services relating to all developments with a highways element. This can include the creation of new highway or the modification of existing highway which would be covered under Sections 38 & 278 of the Highways Act 1980.

The ICS Section can guarantee adoptable designs first time for all highway layouts and provide the latest adoptable construction criteria. This saves the iteration that can occur from multiple design submissions to meet adoptable requirements.

The ICS section can provide design services on the following:-

- Highways
- Roundabouts
- Junctions
- Estate Access and Egress
- Traffic Calming
- Signal Junctions
- Controlled and Uncontrolled Crossings
- Highway Retaining Structures
- Highway Drainage

In addition to the above ICS can provide a Street Lighting design through their PFI Partners, Aurora.

The package ICS provide will guarantee adoptable standards of any scheme, first time. Furthermore should SCCs Highways Operations Section be contracted to carry out the construction works then a Bond of Surety will not be required in relation to the required Highways Agreement.

For further details contact ICS services.

12 Submitting a planning application and engagement with development management

12.1 Pre-applications

The council encourages applicants to seek early engagement with officers from Development Management to discuss proposals for development at SSGA prior to the submission of a planning application, at the pre-application stage. This service helps to speed up the development process and to avoid the submission of unacceptable proposals.

It is recommended that the applicant considers the viability of a site at the pre-application stage, to allow any issues to be resolved before the submission of a formal planning application. Heads of Terms of Section 106s should also be discussed at this stage.

12.2 Planning performance agreement

A Planning Performance Agreement (PPA) is a framework agreed between the City Council and an applicant for the management of complex development proposals within the planning process. A PPA allows both the developer and the council to agree a project plan and programme which will include the appropriate resources necessary to determine the planning application to a firm timetable.

It may be to the advantage of the developer and the City Council that applicants within SSGA enter into a Planning Performance Agreement, due to the scale, nature and complexity of the planning applications likely to be submitted. Further details on the City Council's PPAs can be found on the council's website.

12.3 Planning application requirements

A list of information to be submitted as part of an outline application for development proposals at SSGA are provided below. It should be noted that this list is not exhaustive and further requirements may be identified as a result of pre-application discussions. Some elements may also be more appropriately submitted at reserved matters stage dependent upon the scope of the application made at outline application stage. The Tyne & Wear Validation Checklist will outline the requirements

that must be met before a planning application can be validated.

- Planning Statement
- Design and Access Statement
- Affordable Housing Statement
- Transport Assessment/Transport Statement/Travel Plan/Parking Assessment
- Flood Risk & Drainage Assessment
- Ecological Survey Assessment and Mitigation Report & Protected Species Survey
- Sustainability Statement
- Infrastructure Strategy
- Statement of Community Involvement
- Open Space, Landscape and Recreation Strategy
- Habitat Regulations Assessment/Screening Report
- Environmental Impact Assessment (to include an Environmental Statement)
- Planning Obligations – Draft Head of Terms

There may be a need for additional assessments/statements depending on the site specifics, these may include:

- Heritage Statement
- Land Contamination Assessment
- Noise Assessment
- Air Assessment
- Highways and Public Rights of Way
- New Highways
- Tree Survey and/or Statement of Arboriculture Implications of Development
- Health Impact Assessment
- Coal mining Risk Assessment
- Archaeology study/assessment

12.4 Design review

Design Review is a respected method of improving the quality of new development by offering constructive, impartial and expert advice. Design Review panel meetings allow local authorities, clients, developers and design teams to present their schemes at the pre-planning stage to a panel of experts from the built environment sector and benefit from the discussion and constructive advice of the panel. The Design Review Panel consists of experienced and well respected professionals from a range of built environment disciplines. The Design Review panel is affiliated with Design Council CABE.

Design Reviews are endorsed through the NPPF, as such development proposals at SSGA will be requested to take their proposals to a design north east Design

Review Panel.

Further information is available at:

www.designnetwork.org.uk

12.5 Monitoring

Given the scale and nature of the sites within the SSGA and the complexities associated with the terms and conditions of Section 106s, monitoring charges will be sought. For further details on the charges contact Development Management.

Appendix 1

List of evidence bases

Ecological Evidence Base for Sunderland's Local Development Framework Version 1, SCC (2012) Phase 1 Habitat Study, TNEI (2013/14)

Landscape Character Assessment, (2018) Greenspace Audit and Report, SCC (2013) Listed Buildings Register, SCC

Tyne and Wear Historic Environment Record, Tyne and Wear Specialist Conservation Team

Pupil Forecast Numbers- Education survey, SCC Sustainable Neighbourhoods, SCC (2010) Playing Pitch Plan 2014-19, SCC (2015)

Playing Pitch Plan updated February 2018

Play and Urban Games (PUGS): Delivery Plan 2013-15, SCC (2013)

SHIM Model Testing and Appraisal of the Local Plan Development Sites including SSGA, Jacobs (2014) Metro Strategy 2030 Draft Summary Consultation Document, Nexus (2014)

Renewable Energy Study SSGA, TNEI (2014)

Strategic Flood Risk Assessment, JBA Consulting (2010) Flood Map for Surface Water, Environment Agency (2014)

South Sunderland Growth Area SPD: HRA Screening Report, URS (2014) South Sunderland Growth Area SPD: Appropriate Assessment, URS (2015)

South Sunderland Growth Area SPD: Sustainability Appraisal Scoping Report, URS (2014)

South Sunderland Growth Area SPD: Sustainability Appraisal, URS (2015)

Sunderland Strategic Housing Market Assessment Update (2017)

Appendix 2

Historic environment records

The known archaeological features within the red line are:

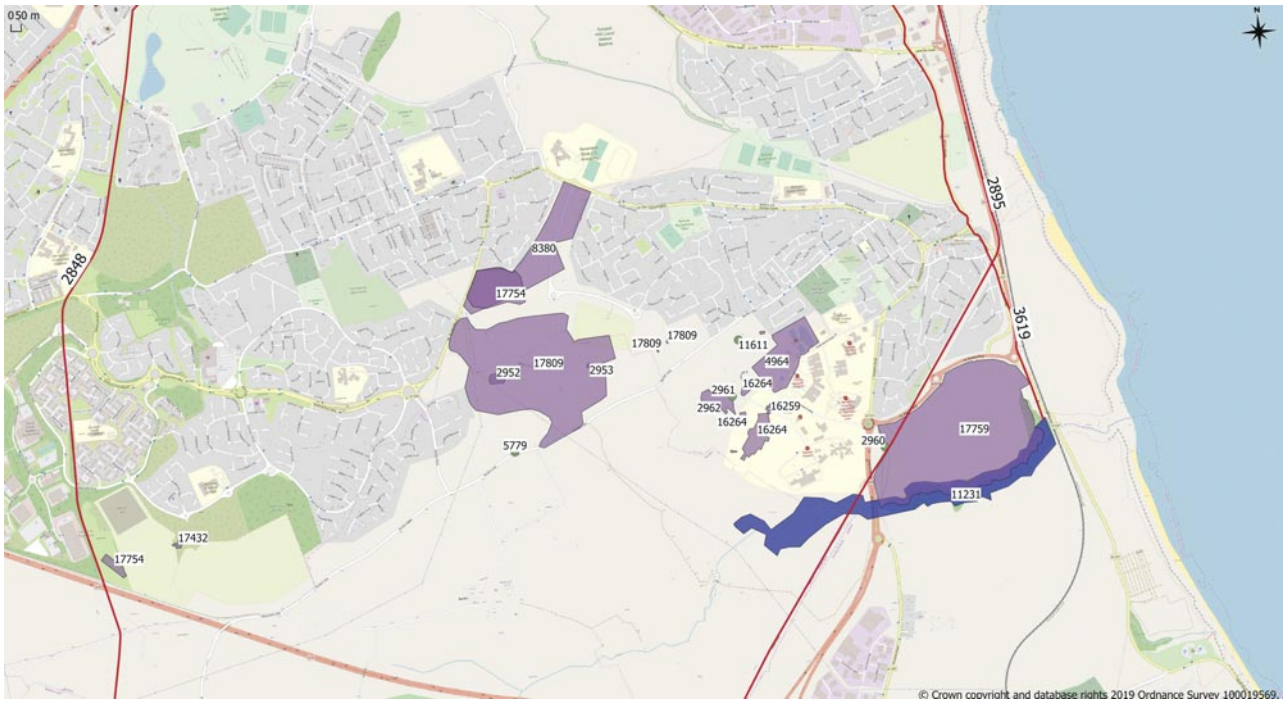
HER 11611	site of Ryhope windmill (shown on a plan of 1869)
HER 2961	site of lime kiln
HER 2960	Stockton Bridge (road bridge over North Eastern Railway)
HER 2895	N.E.R, Durham and Sunderland Branch (North)
HER 5779	site of WW2 roadblock
HER 2848	Hetton Company's Railway
HER 2952	Burdon, Limestone Quarry
HER 2953	Burdon, Limestone Quarry
HER 2962	Ryhope, Limestone Quarries
HER 3619	Sunderland, Sunderland to Stockton Road
HER 8380	Tunstall, ridge and furrow
HER 11610	Ryhope, Mill Hill, Mill House
HER 16259	Ryhope, Field House
HER 16264	Ryhope, Cherry Knowle Hospital
HER 17432	Sunderland, Silksworth, South Moor
HER 17809	Burdon Lane, pre-historic remains
HER 17759	Ryhope, ridge and furrow

The eastern boundary of the site is HER 2894 Londonderry, Seaham and Sunderland Railway

Just outside the site is Ryhope Pumping Station (Scheduled Ancient Monument TW32)

HER 2963	Limestone quarry
HER 2964	Limestone quarry
HER 11231	Ryhope Dene and Cherry Knowle Dene Inventory of Ancient Woodland

Historic Environment Records



Appendix 3

SSAANGs

The following is intended to provide general guidance on the council's approach to SSAANGs.

What land can be used as SSAANGs?

SSAANGs may be created from:

- existing open space of SSAANGs quality with no existing public access or limited public access, which for the purposes of mitigation could be made fully accessible to the public
- existing open space which is already accessible but which could be changed in character so that it is more attractive to the specific group of visitors who might otherwise visit the SPA
- land in other uses which could be converted into SSAANGs.

SSAANGs should not include designated sites or sites of high nature conservation value which are likely to be damaged by increased visitor numbers. Such damage may arise, for example, from increased disturbance and erosion.

Design of SSAANGs

The effectiveness of SSAANGs as mitigation will depend upon their design.

The following describes the design and features expected from the SSAANGs within SSGA. They do not address nor preclude the other functions of green space. Other functions may be provided within SSAANGs, as long as this does not conflict with the specific function of mitigating visitor impacts on the SPA.

Size/Scale of SSAANGs

SSAANGs should be provided at a minimum of 8ha per 1000 population or 18ha per 1000 dwellings. SSAANGs should be split over no more than one or two sites to ensure they are of an appropriate scale to ensure plenty of space is provided for dogs to exercise freely and safely. Numerous small pockets of greenspaces will not be accepted as SSAANGs

Paths

Must be surfaced but not tarmac paths that blend in well with the landscape to avoid the site becoming too urban in feel.

Should be routed so that they are perceived as safe by the users, with some routes being through relatively open (visible) terrain (with no trees or scrub, or well spaced mature trees, or with vegetation back from the path).

Planting

SSAANGs must be perceived as semi-natural spaces with plenty of variation, with areas of open (non-wooded) countryside and areas of dense and scattered trees and shrubs (similar to a country park). SSAANGs should provide a variety of habitats for users to experience.

Water

Water features, particularly ponds and lakes are desirable but not essential and act as a focus for visitors for their visit. The provision of open water on part of, but not the majority of a site is preferable

Furniture

Where possible it is desirable to have a focal point such as a view point, monument etc within the SSAANGs.

Discretely placed benches or information panels along some routes would be acceptable. Information panels to be installed by the council for purposes of consistency amongst the various SSAANGs, green links, corridors

Visually-sensitive way-markers and some benches are acceptable.

Surveillance

SSAANGs should be designed in such a way as to ensure that they benefit from the high levels of natural surveillance. This consideration should inform the choice and position of planting and furniture elements.

Accessibility

SSAANGs where possible should connect to one another through a network of green links and corridors. Every effort should be made from those sites that do not include a SSAANG to link to proposed Green Links/SSAANGs.

Walking routes should be provided within individual SSAANGs, these ideally should connect with other green corridors/links and SSAANGs to allow for circular routes of various lengths.

Access within the SSAANGs must be largely unrestricted with plenty of space provided where it is possible for dogs to exercise freely and safely off lead.

SSAANGs should be clearly sign-posted or advertised in some way.

SSAANGs should have leaflets and/or websites advertising their location to potential users.

Phasing/delivery of SSAANGs

SSAANGs must be operational prior to the occupation of new residential development to ensure that the interests of the SPA are not damaged, unless a phased approach is agreed appropriate with the Council and Natural England.

Maintenance of SSAANGs

SSAANGs must be maintained in perpetuity for a minimum of 75 years. For this reason preference is for the developer to implement the SSAANG and the council to adopt with a commuted sum for 20 years after which the council will maintain in perpetuity:

If the SSAANG is not to be maintained by the LA a Section 106 will be required which;

- prevents the SSAANG being used for any other purpose
- requires the SSAANG to be maintained to a standard agreed by the council
- requires SSAANGs to be maintained and useable for a minimum of 75 years.

Commuted sums

Where a SSAANG is to be maintained by the council, the level of commuted sum required for the maintenance of

the SSAANG will be calculated on a site-by-site basis. The sum required will depend on the type of planting schemes, the quantity of planting, the typography and the costs of works required to bring the site up to the required, adoptable standard.

Appendix 4

SAMMs

Proposed HRA Mitigation Measures (Within each group, mitigation is roughly ranked with higher priorities towards the top)

Proposal	Activity	Frequency	Time Frame	Estimated costs*	Partners**	Implementation**
SCC- existing planned mitigation measures to be implemented by SCC with no cost to developers						
Promote use of alternative beach locations/cliff tops and discourage access to European sites.	North Hendon improvements. Currently low grade pedestrian access. Install visitor signs, improve appearance of tunnel/approach and accessibility, since not in European sites. Install information panels and benches etc.; encourage use of cliff-top England Coast Path.	N/A	Short Term due to be implemented 2014/15	No cost to developers, to be implemented as part of a council project	Network Rail	SCC, Network Rail
	Cliff-top improvements only at Toll Bar/Salterfen. No improvements to beach access (already limited by cliff retreat). Promote cliff tops and England Coast Path rather than beach. Install interpretation panels and benches on the cliff tops.	N/A	Short Term due to be implemented 2014/15	No cost to developers, to be implemented as part of a council project	N/A	SCC- Rights of Way Officer
	Cliff-top improvements only at Village Dene/Beach Road. No improvements to beach access. Access management to dene. Promote cliff tops and England Coast Path rather than beach. Install interpretation panels and benches on cliff tops.	N/A	Short Term due to be implemented 2014/15	No cost to developers, to be implemented as part of a council project	N/A	SCC- Rights of Way Officer
	Already limited access and no formal parking at Ryhope Dene: continue to limit site promotion and access (cannot block since provides emergency beach exit).	N/A	N/A	No cost	NE, ChC, SCC	SCC
Raise awareness at the coast of the European sites.	Install 8 information panels along the coast between Hendon and Ryhope Denemouth to increase awareness of purpose and value of the European sites, provide information on qualifying features, and critically to explain expected visitor behaviour.	Installed as a priority	To be implemented 2014/15	No cost to developers for design and implementation. Installed as part of a council project. Maintenance to be funded by council	Durham Heritage Coast, Public Rights of Way Officer	SCC- Scrutiny
Beach Watch including 'Friends of the Coast'	Volunteers/Police/SCC Responsive Local Services will be policing the area. Volunteers to be trained as walk leaders, promote responsible walking and police irresponsible behaviour.	N/A	Short Term due to be implemented 2014/15	No cost to developers, to be implemented as part of a council project	N/A	SCC

Proposal	Activity	Frequency	Time Frame	Estimated costs*	Partners**	Implementation**
SSGA- Strategic Access Management						
Restrict activities that are highly disturbing to SPA wintering birds.	Public Space Protection Order – Restriction to keep dogs on lead September-April in relevant SPA sections. Fines to be enforced if Coastal Ranger (see below) deems necessary.	Must be reviewed every 3 years, but will be re-instated on each review so continuous in perpetuity (case law suggests 7.5yr).	Prior to residential developments being inhabited, to avoid bad habits forming	Initial implementation costs approximately £3,000. Reduced costs for reinstatement at required 3-yearly review. Approximate cost £40,500 in total. Enforcement costs covered by appointment of Coastal Ranger	SCC	SCC- RLS/Natural Heritage/Planning Implementation/Coastal Ranger
	By-laws to restrict quad bikes/motor bikes/motorised planes and shooting. Restrictions already in place except for motorised planes; strengthen through by-law and officer to enforce.	Must be reviewed every 3 years, but will be re-instated on each review so continuous in perpetuity (case law suggests 7.5yr).	Prior to residential developments being inhabited, to avoid bad habits forming	Initial implementation costs approximately £3,000. Reduced costs for reinstatement at required 3-yearly review. Approximate cost £40,500 in total. Enforcement costs covered by appointment of Coastal Ranger	SCC	SCC- RLS/Natural Heritage/Planning Implementation/Coastal Ranger
Appoint coastal ranger/dog warden to raise awareness and provide support programme in local and wider community.	Organise events	Permanent full time post	Short/Medium Term	£31,259 per annum* (including on costs at current costs) + budget for officer £20,000 per annum = £51,259 per annum (presumption of 20 years after which time the volunteer groups should be in place)= £1,025,180	SCC, Developers at SSGA local schools and community groups, NE	SCC
	Organise publications & walking routes					
	Oversee information panels					
	Organise volunteer service					
	Educate local community					
	Wardening of European sites					
Enforce Public Space Protection Order/By-law (see above)						

*NB Increase in cost due to SCC Workforce Transformation

Proposal	Activity	Frequency	Time Frame	Estimated costs*	Partners**	Implementation**
SSGA- SSAANG and other green infrastructure						
Provide large areas of South Sunderland Areas of Additional Natural Greenspace (SSAANG), connected to other green infrastructure, to draw dog-walkers in particular away from the coast.	8ha SSAANG per 1000 population to be provided in Chapelgarth, Land North of Burdon Lane and Cherry Knowle. To remain in perpetuity. Appropriately designed for dog-walkers in particular, allowing dogs off-lead, providing circular walks and following Natural England SSAANG guidance (see above report text). Land North of Burdon Lane SSAANG to be connected via footpaths to adjacent green infrastructure further north (where amenities such as playing fields are to be installed).	N/A	In advance of residential development being inhabited.	Developer to implement in Chapelgarth, Land North of Burdon Lane and Cherry Knowle. Council preference is for council to adopt the SSAANG with developer providing commuted sum for its maintenance for 20 years (estimated to be approximately £2,147 ¹ per dwelling) after which the council will maintain it in perpetuity.	Developers at SSGA, SCC	SCC
Green infrastructure (GI) improvements	Route 1 Mineral Line improvements. This dismantled railway will be upgraded and built to an adoptable standard.	N/A	Implemented as part of planning applications	Developer to implement	Developers at SSGA, SCC	Developers at SSGA, SCC
	New green link provision. Provision of green link from Cherry Knowle SSAANG to Route 1 Mineral Line green link.	N/A	Implemented as part of planning applications	Developer to implement	Developers at SSGA	Developers at SSGA, SCC
	New green link provision. Provision of green link from Land North of Burdon Lane to Chapelgarth (partly via Land at Burdon Lane) and pedestrianisation of parts of Burdon Lane.	N/A	Implemented as part of planning applications	Developer to implement	Developers at SSGA	Developers at SSGA, SCC
	New green link provision. Provision of green link through Silksworth Lane to maintain existing green corridor.	N/A	Implemented as part of planning applications	Developer to implement	Developers at SSGA	Developers at SSGA, SCC
	Chapelgarth improvements. Upgrading of existing Blackney Woods path and inclusion of lighting. Rationalisation of other informal paths.	N/A	Short/Medium Term	Estimated costs £252000. Fees included	N/A	SCC
	National Cycle Network Route 1 improvements. Link already in place – provide any improvements that maybe necessary.	As and when required.	As and when required.	N/A	Sustrans	Sustrans

¹ Calculated as follows: (amount of SSAANG in ha (q 8ha per 1,000 population) x (£5,912 maintenance per ha per year) x (20 years)/(no. dwellings))

Proposal	Activity	Frequency	Time Frame	Estimated costs*	Partners**	Implementation**
Raise awareness of SSAANG/GI	Installation and maintenance of 12 information panels in SSAANGs and along GI corridors, explaining layout of SSAANG/GI, walking routes, dog-friendly purpose and where dogs are allowed off-lead.	N/A	In advance of residential development being inhabited.	Developers to Implement 7 panels within SSAANGs, 5 to implemented outside of SSAANGs (£2150 per panel + £1000 maintenance = £15,750)	N/A	SCC
	Publicity of dog-friendly SSAANG through website(s), provision of leaflets to new houses, and coastal ranger (see above for latter).	Website information on-going; leaflets provided to new residents.	Website information in advance of residential development being inhabited; leaflets when new residents move in.	Costs covered above (coastal ranger's budget)	N/A	SCC
SSGA - Bespoke additional access mitigation at South Ryhope LMD						
Rationalisation of access to European sites via Ryhope Dene Mouth (LWS).	Access Management into the Dene Mouth	N/A	In advance of residential development being inhabited.	Developer/Landowner to implement	Developers at SSGA, SCC, ChC	ChC and SCC Rights of Way officer
Rationalisation of access to European sites via Ryhope Dene (LWS).	Strong boundary treatment along southern edge of the LMD; a wet ditch, followed by bund with 'established' blackthorn hedges planted.	N/A	In advance of residential development being inhabited.	Developer to implement	Developers at SSGA, SCC, ChC	Developers at S. Ryhope, SCC
SSGA - Monitoring and follow-on mitigation						
Monitor the nature conservation interest of the European sites, to trigger follow-on mitigation if necessary	Undertake surveys of SPA wintering birds and food sources within 6km catchment	9 surveys in first 20 years, once every 5 years for the subsequent 20 years starting on year 20.	First winter (for SPA wintering birds) or summer (for SAC vegetation) prior to occupation of SSGA and then as described to left.	£10,000 per survey, total of £140,000 required	NE, DCC, RSPB, Durham Heritage Coast	SCC- project manage, procure and appoint consultants to undertake these works/surveys
	Undertake surveys of recreational disturbance on SPA wintering birds			£7,000 per survey, total of £98,000 required		
	Undertake condition assessment of SAC vegetation within 6km catchment					
Visitor surveys	Undertake survey of visitor pressure on SAC vegetation within 6km catchment					
	Survey SSAANG users and coastal surveys; improve publicity and make alterations if deemed necessary.	9 surveys in first 20 years, once every 5 years for the subsequent 20 years starting on year 20.	When SSAANG completed and as described to left.	£3,000 per survey, equal to £42,000	N/A	SCC

Proposal	Activity	Frequency	Time Frame	Estimated costs*	Partners**	Implementation**
Further restrictions on activities disturbing to SPA wintering birds (if monitoring indicates required)	Upgrade Public Space Protection Order from dog-leash restriction to dog bans, in winter if, when and where monitoring indicates necessary. Fines to be enforced.	As for dog-leash restrictions above.	If and when triggered by survey monitoring or coastal ranger.	This measure incurs no additional cost because it only requires adjustment to the existing dog-leash restrictions (see above).	N/A	SCC- Responsive Local Services/Natural Heritages/Planning Implementation/Coastal Ranger
Access restrictions to European sites (if monitoring indicates required)	Implement full access restrictions (signage, fencing, monitoring as appropriate) to sensitive areas of European sites if, when and where monitoring indicates necessary.	On-going or temporary in accordance with monitoring and subject to review	If and when triggered by survey monitoring or coastal ranger.	Approximate costs £20,000	NE, DCC, Durham Heritage Coast, Sunderland Coastal Strategy	SCC- Natural Heritage Team and Responsive Local Services in short term until a warden is employed.
SSGA- Administration						
Keep NE and SCC (as the competent authority) informed of the success of mitigation measures	Reports to NE, reporting on the success of mitigation measures	To align with the time frames on monitoring	N/A	See below	N/A	SCC
Provide resources for SCC to administer/implement many of the above measures	Fund SCC officer support time, to administer/implement/oversee/monitor mitigation measures and warden.	On going	N/A	5% of the above total costs (£83,696.50)	N/A	SCC
Total						£1,757,626.50

Acknowledgements

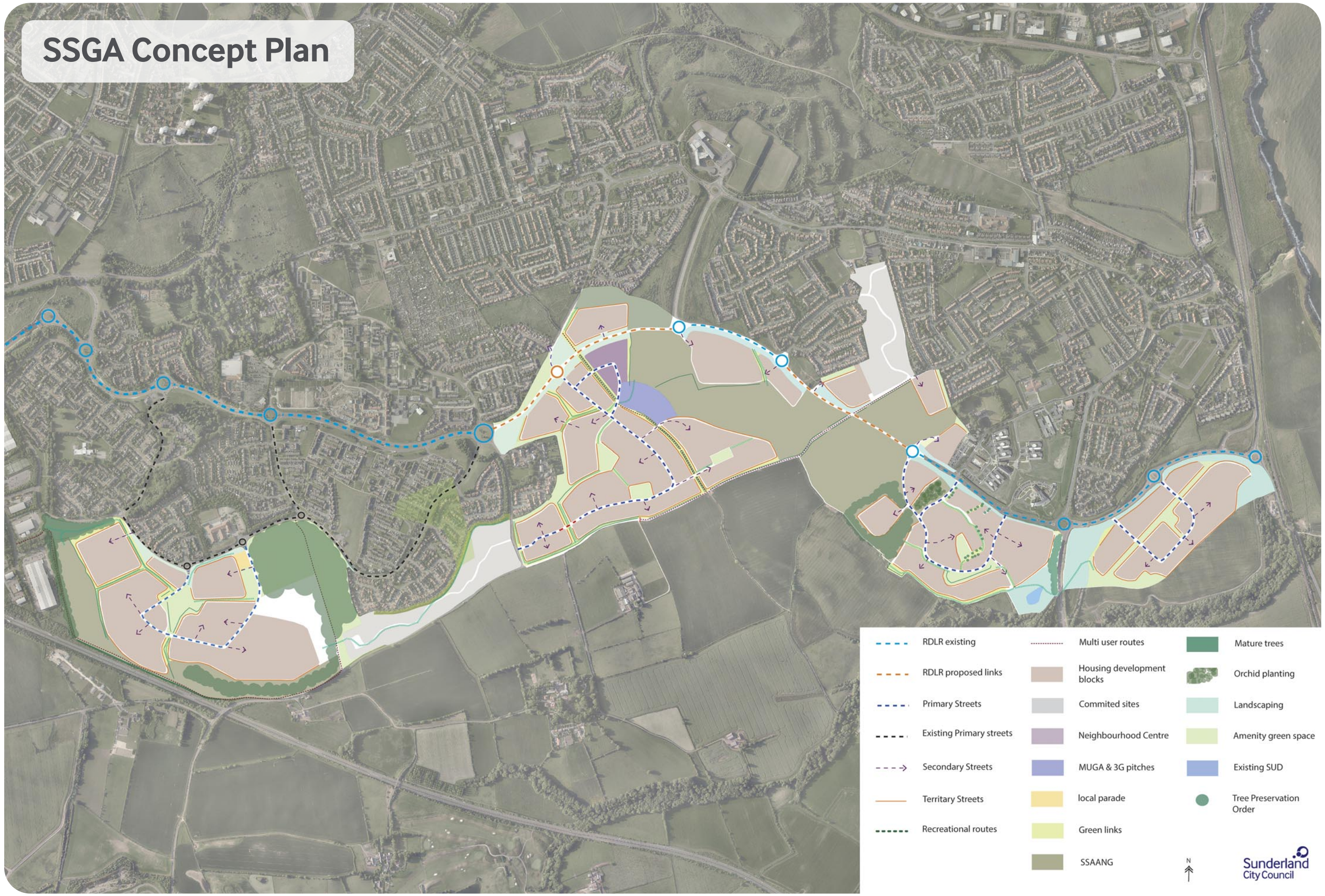
Acknowledgements to the following organisations/companies for use of their images within this document

- SIG Design & Technology - Green Roofs Image
- Riverside, DK-Architects and Shove Media - Extra Care Image
- Paul Eccleston Arthouse Ltd. courtesy of Countryside
- Design for Homes - Numerous Images
- Tim Ronalds Architects - Bunhill Energy Centre Image
- Self Build Portal - Self Build Image
- GSA Studios - Local Centre Image

Appendix 5

Expanded maps

SSGA Concept Plan



RDLR existing	Multi user routes	Mature trees
RDLR proposed links	Housing development blocks	Orchid planting
Primary Streets	Committed sites	Landscaping
Existing Primary streets	Neighbourhood Centre	Amenity green space
Secondary Streets	MUGA & 3G pitches	Existing SUD
Territorial Streets	local parade	Tree Preservation Order
Recreational routes	Green links	
	SSAANG	



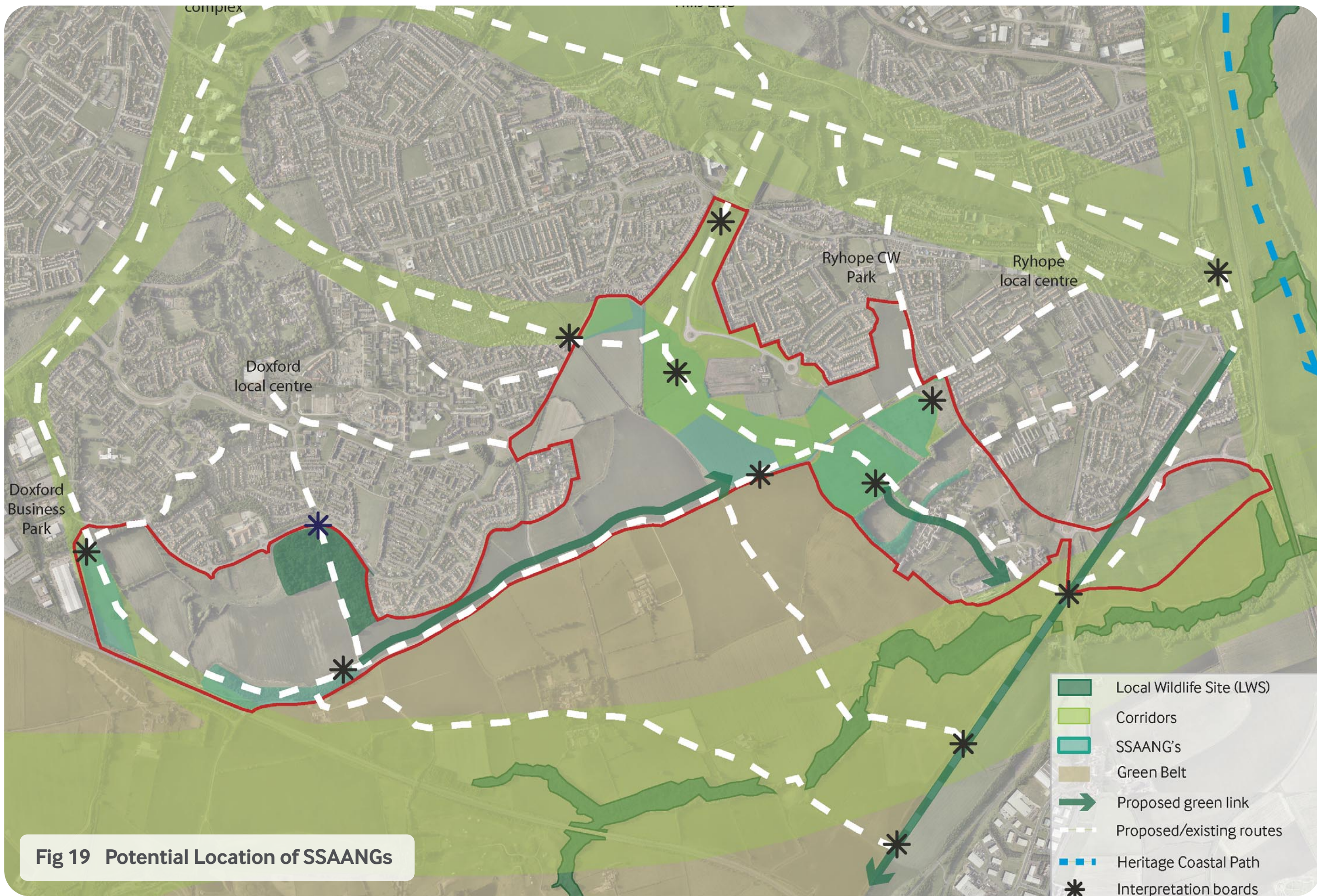


Fig 19 Potential Location of SSAANGs



Fig 22 SSGA Masterplan

	RDLR existing		Territory Streets		SSAANG
	RDLR proposed links		Multi user routes		Mature trees
	Bus only routes		Recreational routes		Orchid planting
	Primary Streets		Neighbourhood Centre		Tree Preservation Order
	Existing Primary streets		MUGA & 3G pitches		Landscaping
	Secondary Streets				Amenity green space

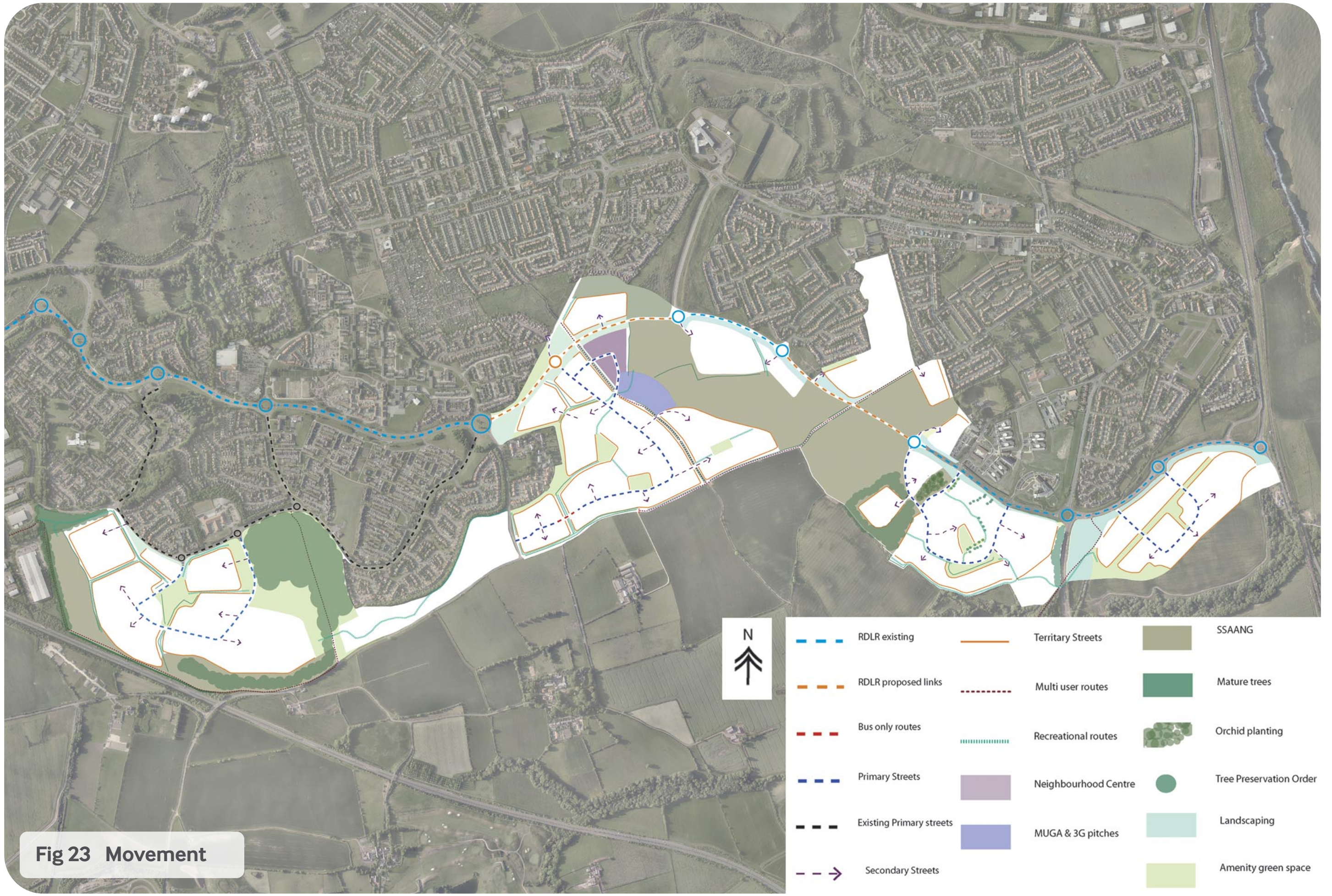


Fig 23 Movement

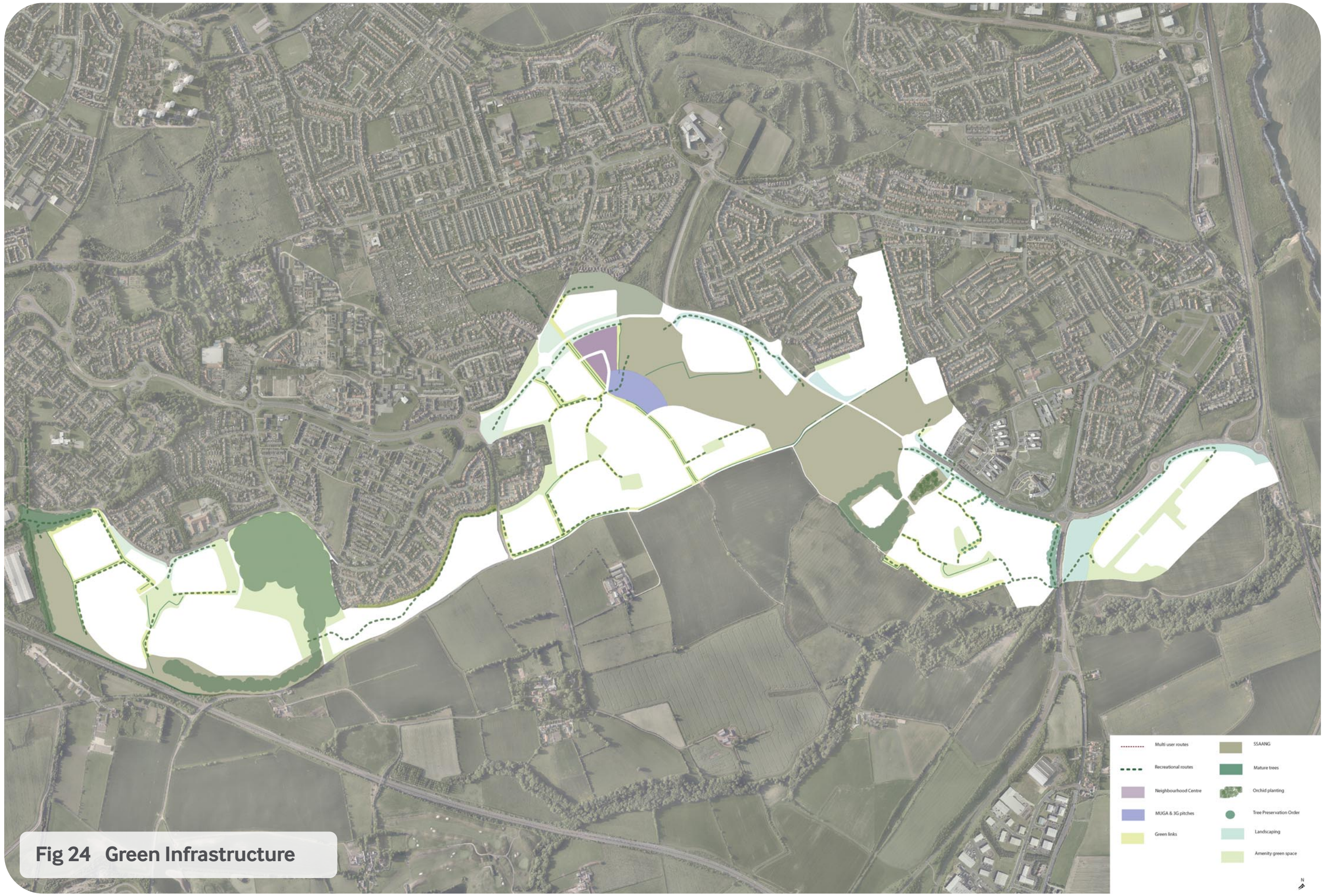


Fig 24 Green Infrastructure

